

*Stratford  
Townscape Study*

STRATFORD TOWNSCAPE STUDY

for the

SHIRE OF AVON

December 1993

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# STRATFORD TOWNSCAPE STUDY

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## **1.0 INTRODUCTION AND SUMMARY**

### **1.1 Introduction**

This study was commissioned by the Shire of Avon Council in December 1992, and has been developed with the assistance of the Townscape Working Committee, with whom we have consulted. Their assistance is gratefully acknowledged.

A Townscape Study is required for Stratford, the study area encompassing the streetscape commencing from the Avon bridge to the south, including Tyers Street and the Princes Highway, to Dawson Road to the north. The northern approaches and the industrial estate are also included. The study is envisaged to provide a framework for future development in the township, as well as improving its general amenity.

The study builds on the outstanding natural environment of the Avon River, and seeks to build on the character present within the townscape. Design also seeks to satisfy the needs of visitors, but more importantly, those of the local community.



## 1.2 A Summary of the Masterplan

The Masterplan (Plan 2) attempts to consolidate the town centre with improved parking, pedestrian circulation and provide stronger links to the Avon River and its reserves. The recommendations of this plan are as follows (note: numbers in brackets correspond to those on Masterplan):

### Northern Approaches

- Staged removal of existing large mature pine trees from north side of Princes Highway, in front of the industrial estate. Replace with avenue of *Eucalyptus mullerana* (Yellow Stringybark), to form a major "gateway" into Stratford. These will extend to the railway, at which point the more formal *Eucalyptus maculata* (Spotted Gum) would form the final approach to the Town Centre. The town entry sign would be placed in the most strategic position to gain the highest profile.
- Council to provide landscape/siting guidelines and improve estate presentation.

### Dawson Street Precinct

- Create a secondary gateway to the town centre. Remove poor tree species in this precinct, and replant with an avenue of *Eucalyptus maculata* (Spotted Gum) (1).
- SEC lines on northern side of Dawson Street to be bundled, to allow establishment of avenue trees.
- Temporary parking to Bowls Club to be discouraged, with off-street car parking being provided off Raymond Street. Bowls Club main entrance to be encouraged off Raymond Street.
- Provide landscaping treatment to corner of Bowls Club, and new footpath.

### Dawson Street to Raymond Street

An important traffic intersection, with a traffic turning conflict at Tyers Street and Dawson Street. A round-about is introduced as both a traffic control measure and a gateway element to announce the arrival into the Town Centre. The approaches would be treated to reinforce this "gateway" through the use of groundcovers and shrubs.

Stratford Bowling Club is to be redeveloped, with an additional green and off-street parking (30 No.) (3).

Kerb extensions are introduced to Dawson Street as part of the round-a-bout redevelopment, and Raymond Street as a separate exercise. Raymond Street would incorporate landscaped pedestrian refuges, incorporating street trees and crossings.

- Raymond Street East, car parking rationalised with one side accommodating 90° angle parking, and the other parallel.
- Kerb extensions are provided for street trees.
- Bus bays are provided adjacent to the RSL building, with the incorporation of a veranda to the north facing wall, to act as a shelter to bus users (5).
- Provide footpath along east side of Tyers Street to Dawson Street.

#### Raymond Street to Hobson Street

Central focus of Stratford, with the attractive Court House and Post Office, and Memorial Park. An opportunity is presented to undertake streetscape improvements to reduce the conflict of busy through traffic, by the introduction of the following:

- Kerb extensions, with landscaping and street trees *Quercus palustris* (Pin Oak);
- Paperbarks to median to be removed and replaced with *Eucalyptus maculata* (Spotted Gum). The median would be planted out with groundcover roses (8);
- an informal pedestrian crossing point to be provided, connecting the Shire Offices with Memorial Park; and
- it is recommended that the Tyers Street speed limit be reduced to 40 kph to further enhance pedestrian safety.

The Shire Offices frontage is to be improved, with feature paving, trees and facade improvements.

Memorial Park will be greatly upgraded to form an attractive park for both residents and visitors (6). Existing planting is to be rationalised, with insignificant species being removed. Feature evergreen tree species will frame the park, a central oval shaped lawn will be formed, incorporating pathways linked to the War Memorial and a new Gazebo.

The Stratford Memorial Hall (Mechanics Hall) grounds are to be upgraded, with the inclusion of a car park (36 spaces) and landscaping.

Additional overflow parking is provided on the western side of MacFarlane Street, to serve the Memorial Hall.

Hobson Street west incorporates corner kerb extensions, with street trees, and 90° angle parking on both sides. Hobson Street east provides 90° angle parking on both sides. Bus/coach parking is provided, and the grocery store is to have its veranda extended around into Hobson Street, to provide shelter for bus users.

#### Hobson Street to Dixon Street

The streetscape treatment and kerb extensions are continued on from the last precinct, including the provision of a second road crossing .

The single storey shops are to be improved with theme colour schemes and co-ordinated signage. Streetscape improvements also include removal of existing planter tubs.

The Stratford Hotel garden frontage to be upgraded with ornamental plantings, reflecting a country garden.

The open space adjacent to Stratford Station is to be improved with parkland planting, with the space serving as a temporary overflow car park as well as passive open space.

#### Dixon Street to Blackburn Street

The Tyers Street streetscape theme is continued from the previous precincts. Avenue tree planting is proposed for Dixon and Blackburn (west) Streets. Angophora tree planting is to be repeated on western section of Blackburn Street.

Presentation to National Bank, corner of Dixon Street east, to be improved with street trees and feature paving .

Shopfronts to west Tyers Street to be upgraded with veranda, colour schemes and signage improvements.

#### Blackburn Street to Avon River

This precinct provides an important 'gateway' approach to the town centre. This area is to continue the streetscape improvements, on Tyers Street, as described in the previous precincts. Extended kerblines are not proposed, retaining the open aspect of the streetscape.

Street tree planting is proposed within the existing nature reserves.

A future development opportunity exists on the open block of land, north of McMillan Street, fronting Tyers Street. This site should be planned as a 'gateway' feature to the town centre, maintaining existing building setbacks.

A right-hand turn traffic lay-by is proposed into McAlister Street.

The Avon Bridge gateway is to be improved by providing avenue planting, reinforcing the north/Bairnsdale gateway theme, using *Eucalyptus maculata* (Spotted Gum). Town entry sign would be erected at the most strategic location.

The Apex Park entry is to be realigned, with its entrance closer to Princes Highway, and the provision of an information board and park signage.

The Avon River foreshore will require an extensive 'clean up' programme, removing litter and exotic weed growth. A walking trail is proposed, linking the Avon River and the town centre. At the corner of McMillan and Merrick Streets, a viewing station will provide views to the Avon, the former bridging point, and the distant ranges.

The Avon Bridge itself is proposed to have the baluster and railing painted in the streetscape colour scheme.

#### The Railway Reserve

The Railway Reserve, located adjacent to McFarlane Street, is an important open space asset. The precinct can be linked by a pedestrian/bike trail to the Avon River, either by the Hobson/Merrick Street trail, or along McFarlane Street. A further route to be investigated is

access between the private residences and the railway land to McMillan Street and Apex Park.

The open space to the west and south to be developed as native parkland, with an area reserved for overflow car parking, near Hobson Street.

## 2.0 STUDY APPROACH

The following text outlines the study approach to the Stratford Townscape Study. The study area comprises the land north of (and including) Avon River between McMillan Street and Dawson Street, including the Princes Highway to the town entry from Bairnsdale. The main area to which the study shall give priority is the main street area of Stratford, ie: Tyers Street .

A brief introduction is provided on the history, current land use structure, and future development potential. Following this, an analysis of the townscape character is undertaken, and the issues, problems and opportunities are identified. Traffic circulation and parking are also considerations at this stage.

The following process was adopted in reaching the recommendations and design concepts:

- Consultation programme throughout the study.
- Collection of database, aerial photographs, photographic record of the Stratford townscape.
- Undertake shopper/visitor surveys.
- Site analysis of townscape, identification of major issues and constraints.
- Preliminary recommendations and concepts concerning broadscale issues and constraints, presented as Opportunities and Constraints Plan (Plan 1).
- Development of a Townscape Masterplan (Plan 2), including preparation of townscape concepts and elevations (Plan 3).
- Finalise recommendations and design concepts, in report and plan form.
- Design Development drawings (Plans 4 and 5) prepared for preferred townscape option.
- Townscape Study Report.

### 3.0 OBJECTIVES

The following text outlines the objectives of the Stratford Townscape Study.

#### 3.1 Goals and Objectives

The study provides the opportunity to create within the township a strong image by promoting a theme in townscape design. The township must have a strong character, one built on its founding history and its present community aspirations.

The key objectives to address are:

- To retain the environmental quality of Stratford, the township has distinctive lifestyle characteristics and a strong sense of community.
- Creation of a strong unified character within the townscape, providing a sense of identity, which in turn would act as a tourist focus for the township.
- To avoid design which caters only for tourism, introducing foreign elements which are not part of the heritage of the area, at the expense of the local community.
- Improve the visual presentation of the townscape and Avon River foreshore.
- To improve the amenity of the townscape with improvements to on-street/off-street parking and traffic circulation.
- Provision of safe and attractive pedestrian crossings within the town. Solutions can involve 'visual' narrowing of the townscape by landscape improvements, where the zone is seen as a pedestrian priority precinct and further reducing speed limits.
- An assessment and improvement of existing built form, guidelines for signage and advertising, setbacks, architectural controls, and investigation of a townscape theme.
- To provide townscape design drawings, which are practical and economically achievable for the community to undertake. Design would provide details on:
  - footpath upgrading;
  - co-ordinated street furniture; and
  - signage;

## 4.0 THE EXISTING CHARACTER OF STRATFORD



Figure 1 - An aerial view of Stratford

### 4.1 Character Definition

There are a number of aspects of a town which need to be recognised and understood in order to identify its particular character.

A town's character results from a combination of natural and man-made elements and from these we can realise its "sense of place". Many of our later developments have created insensitive urban environments, which generally have a nullifying effect on our town's character. Natural elements such as land form, climate and vegetation can provide a unique set of qualities for urban development to respond to.

#### 4.1.1 History

Stratford has an interesting history in the settlement of the area, commencing in the 1840's as a major river crossing for the Gippsland stock route. It was remarked that "Stratford appears the proper site for a town with church, parsonage, and school, for it is the most central spot in the upper district". The Shakespeare hotel already existed a distance away from the Avon



River ford, and on the old stock route. From the 1850's onward the town of Stratford developed as a "convenience for drovers moving stock from Maneroo."

"Stratford was so-called because there was a good ford at this part of the river (the Avon ), which was of the greatest importance when there were no bridges".

A number of key developments and features are identified:

- Shakespeare Hotel - 1848 ( delicensed in 1886);
- first bridge to cross Avon -1854, destroyed in floods of 1870, and rebuilt in 1874, destroyed again by the floods of 1950, and rebuilt;
- Lloyd's store - 1856;
- further businesses established - 1866;
- Presbyterian church - 1857 (replaced in 1881);
- Roman Catholic church (St.Patrick's) - 1864, new school building - 1929;
- Holy Trinity church - 1868;
- Wesleyan chapel - 1873;
- school - 1870, replaced by new school building - 1911; and
- period of 1880's time of expansion, with establishment Post Office, Courthouse and Shire Offices 1885; Railway reaches Stratford in 1888; Mechanics Hall and Library in 1889.

Figures 2 - 3 provide a glimpse of the built character of Stratford in its early development ( photos provided by the Stratford Historical Society and the Leader Newspaper)

Today, Stratford serves its hinterland as an importantservice town and administrative centre for the Shire of Avon, as well as an important recreational destination, with its riverine parks, the Shakespeare Festival.

#### 4.1.2 Character of Stratford

Stratford has a distinctive setting, residing on the early Gippsland stock route of the early pioneers, and an important bridging point on the Avon River.

Building style is consistent, with low scale single storey buildings, early development using timber weatherboard and later brick. Buildings exceeding single storey are

characterised by the later State Bank building. Single storey buildings form the dominant townscape form, and this can be seen in early photographs of the shops along Tyers Street.

Much of Stratford's character can be traced to its role as a service town for its rural hinterland; its strong association to the Avon River; and the string of destructive floods throughout the history of the settlement.

Another introduced element is the Shakespeare Festival, held in May and is a major attraction to the region.



The Shire office, court house and post office, 1906. (The Leader, 1906)

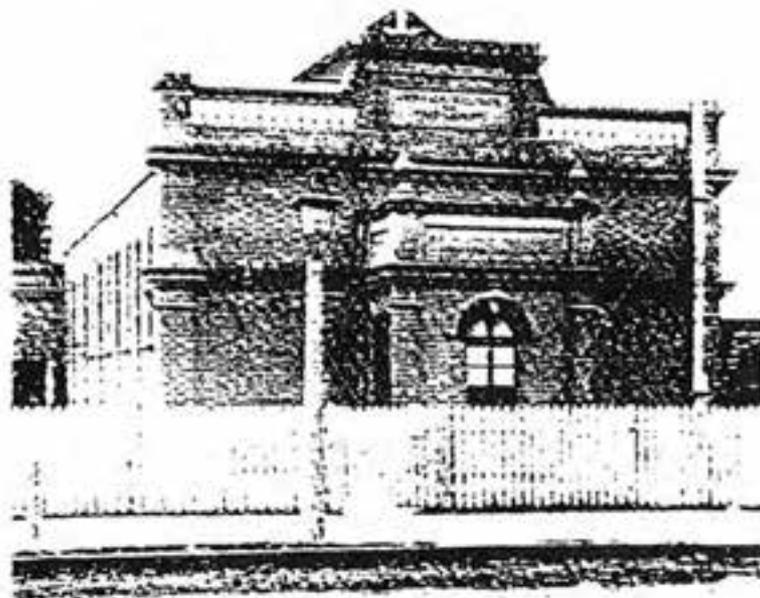


Figure 2

The Peckham's institute, 1906. (The Leader, 1906)

Lloyd's store in later years  
(Stratford Historical Soc.)



Tyers Street, looking north, 1906 (The Leader, 1906)



The parish hall, vicarage and Holy Trinity church, c. 1907. (Flo Pearce)

Figure 3

STRATFORD TOWNSCAPE STUDY

## 4.2 Precinct Description

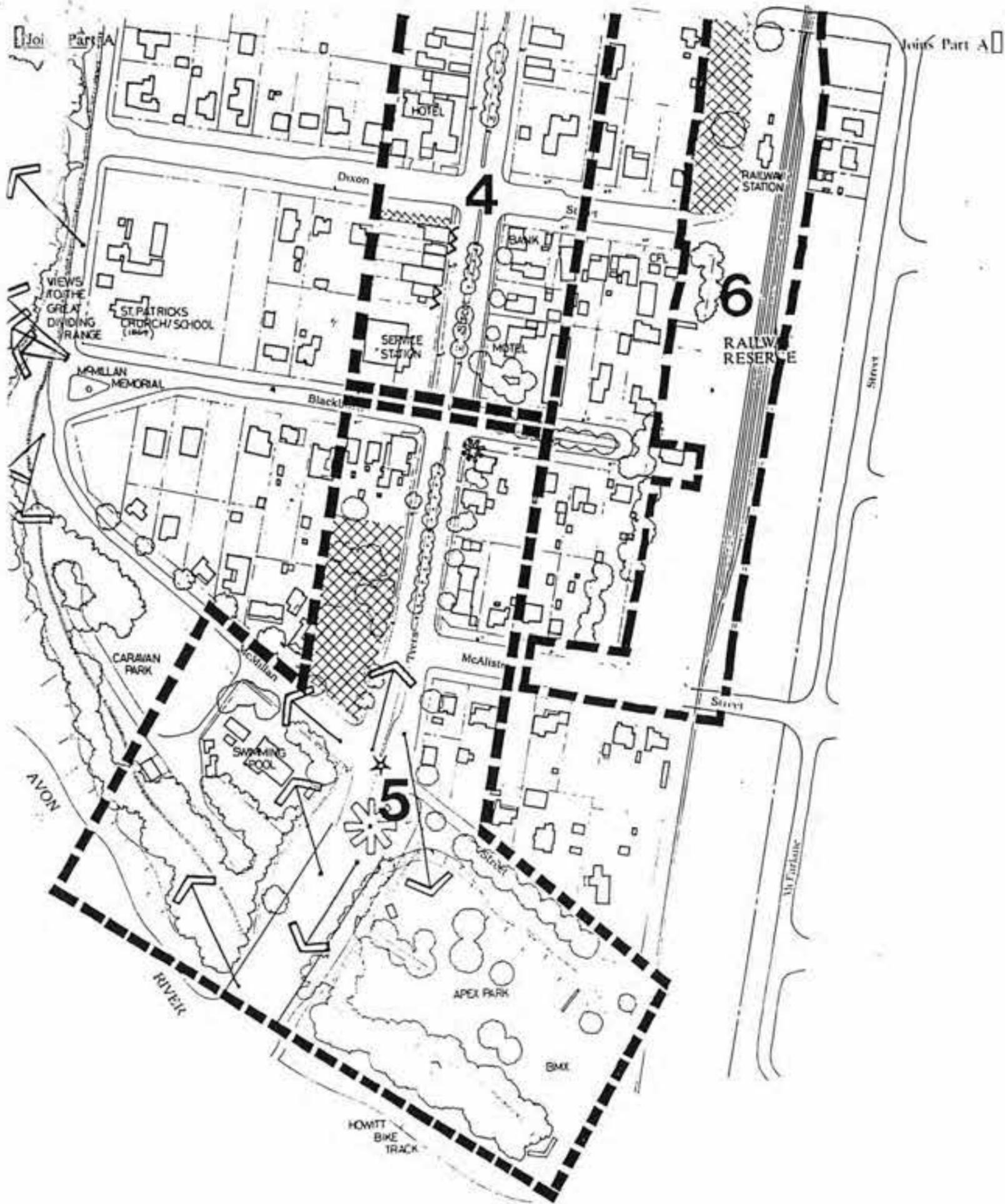
For the purpose of this study, the Townscape Masterplan has identified a number of character precincts which describe in detail the existing conditions and opportunities for improvements (Plan 2).

### 4.2.1 Northern Approach

- The Bairnsdale approach forms an attractive gateway, with the Princes Highway entering Stratford from the elevated cutting.
- This approach forms a transition from a lightly treed rural landscape to the industrial and service activity on the fringe of town.
- There exists an opportunity to upgrade the presentation of this approach with significant avenue tree planting to create a more positive and attractive entry.
- Entry signage to this approach lacks originality and says little of the character of Stratford. Opportunity to up grade to create a more attractive and distinct sense of entry
- The presentation and signage to the buildings in the industrial estate detract from the appearance of this precinct. The signage is generally excessive and building presentation poor, creating an intrusive element. A co-ordinated signage and presentation strategy is required to improve the appearance of this gateway and provide an example of good presentation for future developments in the area.

### 4.2.2 Dawson Street Precinct

- Important secondary gateway to commercial and civic precinct of Stratford.
- Improvements are required to upgrade this entry to the township. existing and future highway uses should contribute positively to this gateway.
- Existing street tree planting to this precinct is inconsistent. Significant street tree planting is required to create a greater sense of entry and concentrate on views along the street.



- LEGEND:**
- Townscape precinct
  - Streetscape character building
  - Streetscape presentation / facility requiring upgrading
  - Dominant views
  - Pedestrian / vehicle conflict point
  - Significant street trees to be retained
  - Insignificant street trees to be removed
  - Gateway / Town Entry
  - Opportunities for open space links
  - Under utilised land with opportunity for re-development / parking
  - Overhead services
    - Power pole
    - Light pole
    - Power pole with light fitting
  - Town frontage requiring sensitive development treatment

## STREETSCAPE PRECINCTS

### COMMERCIAL PRECINCT

- WHILE THERE ARE FEW OUTSTANDING CHARACTER BUILDINGS IN THE COMMERCIAL PRECINCT THE OVERALL APPEARANCE OF THE TOWNSCAPE IS INFLUENCED BY THE GROUPS OF SMALL STOREY SHOPS.
- THE EXISTING POSTED VERANDAS AND PRAPETS ARE A DOMINANT FEATURE AND IMPORTANT CHARACTER ELEMENT OF MANY OF THE SHOPS IN THE COMMERCIAL PRECINCT.
- THE COMMERCIAL PRECINCT LACKS A PARTICULAR FOCUS DUE TO ITS STRIP-LIKE AND THE AD-HOC ARRANGEMENT OF SHOPPING FACILITIES.
- THE EXISTING MELALEUCA TREES TO THE CENTRAL MEDIAN CREATE A VISUAL BARRIER BETWEEN THE EASTERN AND WESTERN SIDES OF THE STREET. THEY ALSO OBSCURE BIGHT LINES FOR PEDESTRIAN CROSSINGS AND OBSCURE VIEWS TO THE SHOPS.
- REMOVAL OF THE EXISTING MELALEUCA TREES TO THE CENTRAL MEDIAN WOULD PROVIDE THE OPPORTUNITY TO ESTABLISH A CLEAR STEWARD OPEN CANOPIED TREE WHICH WOULD PROVIDE GREATER VISIBILITY TO THE STREETSCAPE.
- THE EXISTING IRON GLASS PLANTING TUBES IN TYERS STREET ARE IN POOR CONDITION AND POORLY MAINTAINED. THEY OBSTRUCT THE FOOTPATH AND SHOULD BE REMOVED IN FAVOUR OF ORNAMENTAL PLANTING.
- THERE IS A LACK OF PUBLIC SEATING TO THE TYERS STREET COMMERCIAL PRECINCT. ADDITIONAL PUBLIC SEATING SHOULD BE PROVIDED TO DESIGNATED AREAS OF THE STREETSCAPE.
- THE EXISTING COLOUR SCHEMES AND SIGNAGE TO THE BUILDINGS WITHIN THE COMMERCIAL PRECINCT ARE GENERALLY POOR. A CO-ORDINATED PRESENTATION SCHEME IS REQUIRED FOR THE STREETSCAPE WHICH REFLECTS THE ESTABLISHED CHARACTER OF STRATFORD.
- A NUMBER OF VACANT SITES ARE FOUND IN THE COMMERCIAL PRECINCT WHICH PROVIDE AN OPPORTUNITY FOR FUTURE INFILL DEVELOPMENT. NEW INFILL SHOULD RELATE IN SCALE, FORM, SETBACK AND DETAIL TO THE ESTABLISHED CHARACTER OF STRATFORD.
- RAILWAY RESERVE - STATION PROVIDES FUTURE POTENTIAL AS DEVELOPMENT SITE IF BUS DEPOT / BIKE STORAGE & OVERFLOW CAR PARKING.

### SOUTHERN APPROACH / RECREATION PRECINCT

- THE SMALL TIMBER SHOP TO THE CORNER OF TYERS STREET AND BLACKBURN STREET IS A WELL MAINTAINED EXAMPLE OF AN EARLY VICTORIAN SHOP IMPORTANT TO THE CHARACTER OF THE TOWN.
- THE PRESENTATION OF THE STRATFORD AUTOMOTIVE SERVICES IS EXCELLENT. THE SERVICE BUILDING COLOURS AND LANDSCAPING PROVIDE AN ATTRACTIVE BUILDING WHICH COMPLIMENTS AND ENHANCES THE STREETSCAPE. THIS PRESENTATION SHOULD BE USED AS AN EXAMPLE TO OTHER RETAILERS.
- THE AVON BRIDGE ACROSS THE RIVER CREATES A UNIQUE ENTRY EXPERIENCE TO STRATFORD.
- ADDITIONAL AVENUE STREET TREE PLANTING BETWEEN THE BRIDGE AND THE TOWN CENTRE CAN PROVIDE A GREATER SENSE OF ENTRY TO THE TOWNSHIP.
- EXISTING TREE PLANTING TO McWILLIAM STREET AND BLACKBURN STREET ARE A GOOD EXAMPLE OF THE SENSE OF SCALE CREATED BY LARGE AVENUE TREES. THIS PLANTING TYPE MAY BE REPERFORMED THROUGHOUT THE STRATFORD TOWNSCAPE.
- APEX PARK IS A GOOD OPEN SPACE FACILITY HOWEVER IT SHOULD BE UPGRADED TO FORM A CLOSER LINK WITH THE TOWN AND THE RIVER. PHONE AND PLAY FACILITIES WITHIN THE PARK SHOULD BE UPGRADED AND FOOTPATH ACCESS BETWEEN THE RIVER AND THE TOWNSHIP ALSO IMPROVED.
- THE EXISTING VACANT LAND TO THE WESTERN SIDE OF TYERS STREET, BETWEEN McWILLIAM STREET AND BLACKBURN STREET PROVIDES OPPORTUNITY FOR REDEVELOPMENT AS A MAJOR GATEWAY FACILITY FOR THE TOWN. THE DESIGN AND SCALE OF ANY FUTURE DEVELOPMENT SHOULD RELATE TO THE EXISTING CHARACTER OF STRATFORD.

### RAILWAY RESERVE

- WIDE OPEN AREA TO THE MELBOURNE - BARRHOLME RAILWAY LINE.
- NEW BUILDING ELEMENT OF THE TIMBER WEATHERBOARD STRATFORD STATION ELEVATED POSITION.
- POTENTIAL FOR FUTURE REDEVELOPMENT OF THE AREA AS OPEN SPACE WITH A LINK FROM PARKLANDS TO THE AVON RIVER AND APEX PARK.
- THE OPEN AREA BETWEEN HOBSON AND DIXON STREETS CAN BE REDEVELOPED TO ACCOMMODATE OVERFLOW PARKING FOR THE STRATFORD COMMUNITY HALL AND FESTIVAL ACTIVITIES.
- EAST OF THE RAILWAY LINE PROPOSED FOR FUTURE RECREATING DEVELOPMENT WITH FRONTAGE TO McWILLIAM STREET.



Project  
**STRATFORD TOWNSCAPE STUDY**

Drawn by  
**OPPORTUNITIES & CONSTRAINTS PLAN - PART B**  
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# STREETSCAPE PRECINCTS

## NORTHERN APPROACH

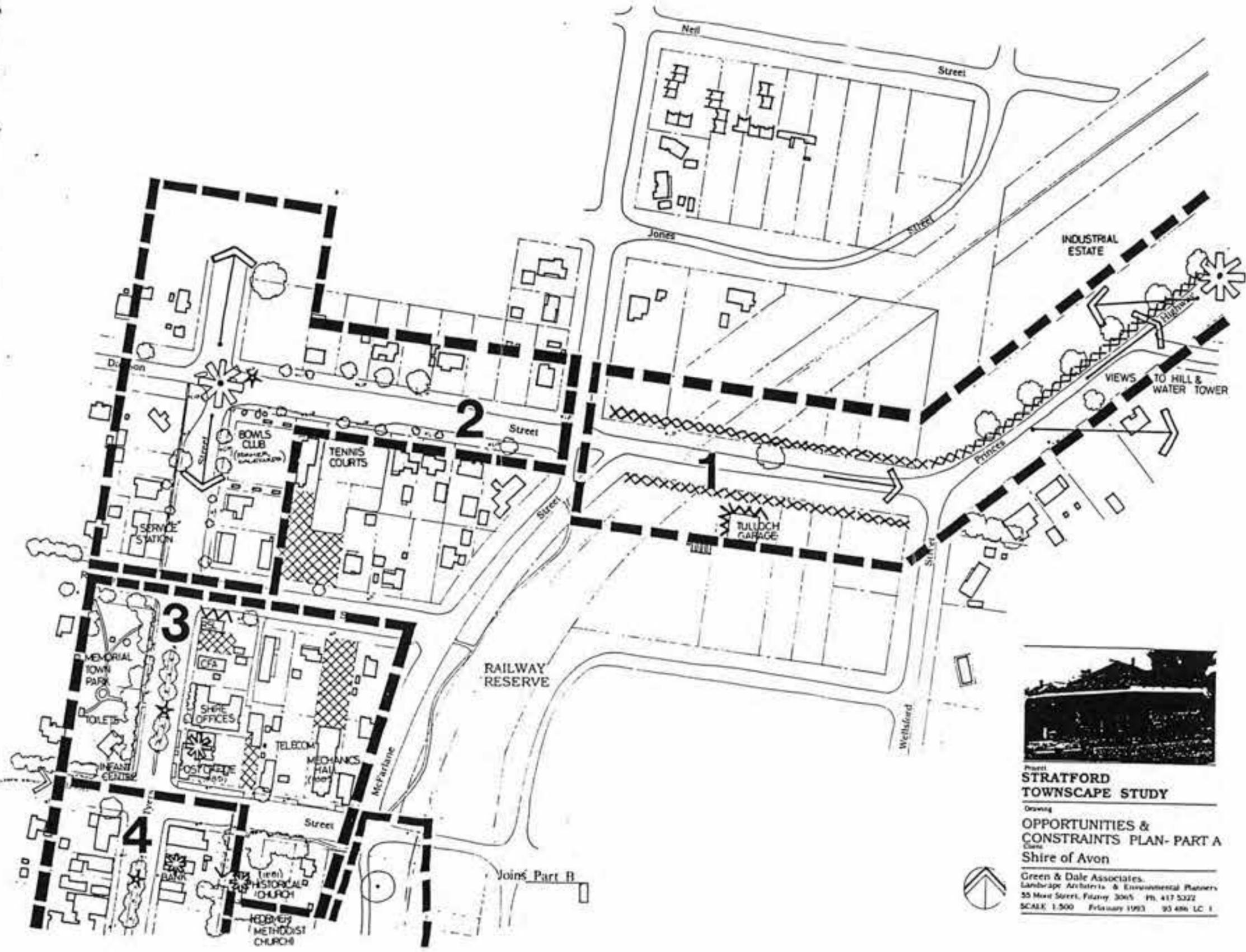
- THE SUPPLEMENTAL APPROACH FORMS AN ATTRACTIVE GATEWAY, WITH THE PRINCE HIGHWAY EXTENDING STRAIGHT FROM THE BLENDED CUTTING.
- THIS APPROACH FORMS A TRANSITION FROM A LIGHTLY TREED RURAL LANDSCAPE TO THE INDUSTRIAL AND SERVICE ACTIVITY OF THE TOWN.
- THERE EXISTS AN OPPORTUNITY TO UPGRADE THE PRESENTATION OF THIS APPROACH WITH SIGNIFICANT AVENUE TREE PLANTING TO CREATE A MORE POSITIVE AND ATTRACTIVE ENTRY.
- ENTRY SIGNS TO THIS APPROACH LACK OPPORTUNITY AND SETS LITTLE OF THE CHARACTER OF STRATFORD. OPPORTUNITY TO UP GRADE TO CREATE A MORE ATTRACTIVE AND OBSTRUCTIVE SIGNAL OF ENTRY.
- THE PRESENTATION AND SIGNALS TO THE BUILDINGS IN THE INDUSTRIAL ESTATE ENTRACT FROM THE APPEARANCE OF THIS PRECINCT. THE SIGNALS IS GENERALLY EXCESSIVE AND BUILDING PRESENTATION POOR, CREATING AN INTRUSIVE ELEMENT. A COORDINATED SIGNAL AND PRESENTATION STRATEGY IS REQUIRED TO IMPROVE THE APPEARANCE OF THIS GATEWAY AND PROVIDE AN EXAMPLE OF GOOD PRESENTATION FOR FUTURE DEVELOPMENTS IN THE AREA.

## DANSON STREET PRECINCT

- IMPORTANT SECONDARY GATEWAY TO COMMERCIAL AND CIVIC PRECINCT OF STRATFORD.
- IMPROVEMENTS ARE REQUIRED TO UPGRADE THE ENTRY TO THE TOWNSHIP. EXISTING AND FUTURE HIGHWAY SIGNS SHOULD CONTRIBUTE POSITIVELY TO THIS GATEWAY.
- EXISTING STREET TREE PLANTING TO THIS PRECINCT IS INCONVENTIONAL. SIGNIFICANT STREET TREE PLANTING IS REQUIRED TO CREATE A GREATER SENSE OF ENTRY AND CONTRASTS ON VIEWS ALONG THE STREET.
- THE EXISTING OVERHEAD POWERLINES TO THE WESTERN SIDE OF TYNE STREET AND THE NORTH SIDE OF DANSON STREET FORM A CONSTRAINT TO TREE PLANTING.
- THE INTERSECTION OF TYNE STREET AND DANSON STREET IS DANGEROUS AND MAY REQUIRE ADDITIONAL TRAFFIC CONTROL TREATMENT TO IMPROVE ACCESS.
- THE ENTRY EXPERIENCE FROM THE NORTHERN APPROACH TO STRATFORD IS REINFORCED BY THE SEQUENCE OF GATEWAYS WHICH INCLUDE THE RAILWAY CROSSING AND THE R/F CORNER FROM DANSON STREET INTO TYNE STREET.
- THE PRESENTATION OF THE BOWLS CLUB CONTRIBUTES TO THE GREEN CHARACTER OF THIS APPROACH TO STRATFORD. HOWEVER ADDITIONAL STREET TREE PLANTING WOULD CONTRIBUTE FURTHER TO THIS CHARACTER.

## CIVIC PRECINCT

- THE STREETSCAPE IN THIS PRECINCT IS LIMITED BY USE AND SCALE OF ITS CIVIC BUILDINGS. FACILITIES FORMED IN THIS PRECINCT INCLUDE THE POST OFFICE, LIBRARY, SHIRE OFFICES, A S.I. HALL, INFANT WELFARE CENTRE AND MECHANICS HALL.
- THE STREETSCAPE CHARACTER OF THIS PRECINCT IS INFLUENCED BY KEY CHARACTER BUILDINGS INCLUDING THE LIBRARY, POST OFFICE, S.I. HALL AND OLD CHURCH.
- THE TOWN PARK IS AN APPROPRIATE FACILITY WITHIN THIS PRECINCT. HOWEVER IT INCLUDES UPGRADE. WHILE THE FACILITIES PROVIDED IN THE PARK ARE ADEQUATE, IT DOES NOT PLAY ITS POSITION AS A FOCUS TO THE TOWN PLANTING WITHIN THE PARK IS POOR AND TREES INTO THE PARK ARE OBLIVIOUS BY THE MELALEUCA TREES PLANTED IN THESE STREET. AN OPPORTUNITY EXISTS TO UP GRADE THIS PARK AS A MAJOR COMMUNITY VISITOR FOCUS.
- THE HIGH VOLUMES OF TRAFFIC ON TYNE STREET ARE A MAJOR CONSTRAINT TO ACCESS BETWEEN THE EAST AND WEST SIDES OF THE STREET. THIS AREA PROVIDES A GOOD OPPORTUNITY FOR A PIECE OF TRAFFIC CROSSING INCLUDING A PICKUP/DROP OFF TO THE CENTRAL MEDIAN.



Joins Part B

Joins Part B



Project  
**STRATFORD TOWNSCAPE STUDY**  
Drawing  
**OPPORTUNITIES & CONSTRAINTS PLAN - PART A**  
Client  
**Shire of Avon**



Green & Dale Associates.  
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SCALE 1:500 February 1993 93 486 LC 1





- The existing overhead powerlines to the western side of Tyers Street and the north side of Dawson Street form a constraint to tree planting.
- The intersection of Tyers Street and Dawson Street is dangerous and may require additional traffic control treatment to improve access to the residential areas.



Figure 4 - Dawson Street looking east.

- The entry experience from the northern approach to Stratford is reinforced by the sequence of gateways which include the railway crossing and the 90 degree corner from Dawson Street into Tyers Street.
- The presentation of the bowls club contributes to the 'green' character of this approach to Stratford, however additional street tree planting would contribute further to this character.

#### 4.2.3 Civic Precinct

- The streetscape in this precinct is unified by use and scale of its civic buildings. Facilities found in this precinct include the Post office, library,

Shire offices, RSL hall, infant welfare centre and Mechanics Hall.

- The streetscape character of this precinct is influenced by key character buildings including the library, post office, RSL and old church.
- The town park is an appropriate facility within this precinct, however it requires upgrading. While the facilities provided in the park are adequate, it does not fulfil its potential as a focus to the town. Planting within the park is poor and views into the park are obscured by the Melaleuca trees planted in Tyers Street.



Figure 5 - Stratford Post Office

- An opportunity exists to up grade this park as a major community/ visitor focus.
- The high volumes of traffic on Tyers street are a major constraint to access between the east and west sides of the street. This area provides a good opportunity for a pedestrian crossing including a pedestrian refuge to the central median.



Figure 6 - The wide expanse of Tyers Street

4.2.4 Commercial Precinct

- While there are few outstanding character buildings in the commercial precinct, the overall appearance of the townscape is influenced by the groups of single storey shops.

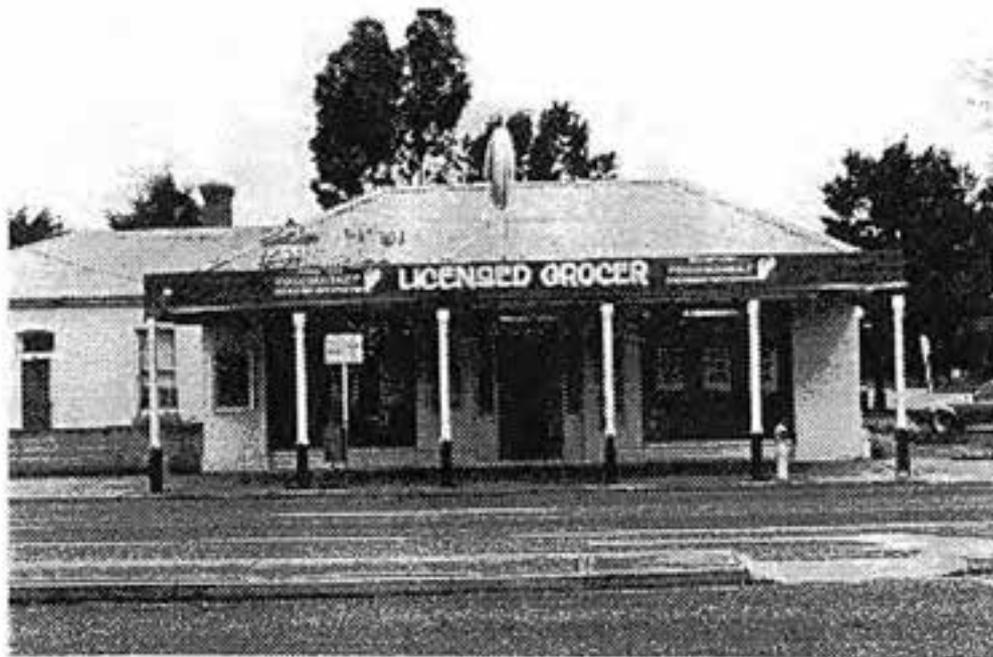


Figure 7 - A good example of post support veranda shopfront in Stratford



Figure 8 - Poor infill development, the National Bank.

- The existing post support verandas and parapets are a dominant feature and important character element of many of the shops in the commercial precinct.
- The commercial precinct lacks a particular focus due to its strip nature and the ad hoc arrangement of shopping facilities.
- The existing Melaleuca trees to the central median create a visual barrier between the eastern and western sides of the street. They also obscure sight lines for pedestrian crossings and obscure views to the shops.
- Removal of the existing melaleuca trees to the central median would provide the opportunity to establish a clear stemmed open canopied tree which would provide greater visibility to the streetscape.



Figure 9 - Paperbarks (Melaleuca) on the Tyers Street median

- The existing fibre glass planting tubs in Tyers Street are in poor condition and inadequately maintained. They obstruct the footpath and should be removed in favour of on ground planting.



Figure 10 - Unattractive planter boxes

- There is a lack of public seating to the Tyers Street commercial precinct. Additional public seating should be provided to designated areas of the streetscape.
- The existing colour schemes and signage to the buildings within the commercial precinct are generally poor. A co-ordinated presentation scheme is required for the streetscape which reflect the established character of Stratford.
- A number of vacant sites are found in the commercial precinct which provide an opportunity for future infill development. New infill should relate in scale, form, setback and detail to the established character of Stratford.

#### 4.2.5 Southern Approach / Recreation Precinct

- The small timber shop to the corner of Tyers Street and Blackburn Street is a well maintained example of an early Victorian shop, important to the character of the town.
- The presentation of the Stratford Automotive Services is excellent. The signage, building colours and landscaping provide an attractive building which compliments and enhances the streetscape. This presentation should be used as an example to other retailers.



Figure 11 - A small Victorian shop provides streetscape character & charm.

- The Avon bridge across the river creates a unique entry experience to Stratford.
- Additional avenue street tree planting between the bridge and the town centre can provide a greater sense of entry to the township.
- Existing tree planting to McMillan Street and Blackburn Street are a good example of the sense of scale created by large avenue trees. This planting type may be reinforced throughout the Stratford townscape.
- Apex Park is a good open space facility however it should be upgraded to form a closer link with the town and the river. Picnic and play facilities within the park should be upgraded, and pedestrian access between the river and the town centre also improved.
- The existing vacant land to the western side of Tyers Street, between McMillan Street and Blackburn Street, provides opportunity for redevelopment as a major gateway facility for the town. The design and scale of any future development should relate to the existing character of Stratford.

#### 4.2.6 Railway Reserve

- Wide linear reserve to the Melbourne - Bairnsdale railway line.
- Main building element of the timber weatherboard Stratford Station. Elevated position.
- Potential for future redevelopment of the area as open space, with a linear parkland link to the Avon River and Apex Park.
- The open area, between Hobson and Dixon Streets, can be redeveloped to incorporate overflow parking for the Stratford Mechanics Hall and festival activities.
- East of the railway line preferred for future residential development, with frontage to McFarlane street.

### 4.3 Traffic and Parking Considerations

#### 4.3.1 Car Parking and Traffic Circulation

Car parking for the town centre is predominantly contained to the Tyers Street (Princes Highway), with overflow parking in Raymond / Hobson / Dixon Streets and in the reserve car parks to the Avon, near McMillan Street.

Parking along Tyers Street consists of parallel parking, largely restricted by traffic flow along the Princes Highway.

Very little shopper parking occurs in the streets parallel to Tyers Street, despite provision of wide streets for parking. Clearly this is due to the area being relatively remote from the town centre, in walking terms.

A number of the streets on survey were found to accommodate parallel parking on one side, and angle parking on the opposite side. This layout does not provide the optimum layout to maximise the number of spaces available. It should be noted that the roadways, which have a kerb-to-kerb width of 18 metres at their narrowest to 20 metres at their widest, is sufficiently wide to allow the angle parking to be modified from 45° to 90°, increasing the yield by up to 30%.

Off - street parking is limited to small areas to the rear of commercial premises, such as the Shire Offices/ Post Office and temporary parking on unused land along McFarlane Street, opposite Stratford Station. Available land is scarce for off-street parking, and will have to be incorporated with future development proposals.

#### Future car parking

The development of additional car parking to supplement existing on-street parking at peak times is desirable. Two real options exist:

- a) Develop and promote existing side street parking areas for shopper use.
- b) Develop car parks at the rear of the shops with access onto Tyers Street.



The first option is considered the most appropriate in the short to medium term because:

- The areas are more visible and are closer to the Town centre.
- Existing car parks within the town could be improved by providing:
  - more visible access via signage and linemarking;
  - defined parking spaces and aisles to increase capacity; and
  - if acceptable, consideration to new off street car park areas to the Bowls Club, the Mechanics Hall, and overflow parking along McFarlane Street, could be given.

A long-term strategy of developing car parks at the rear of shops should be considered to supplement the Tyers Street parking areas. This is probably best achieved in conjunction with future redevelopment proposals, say to the undeveloped site on the corner of Tyers and McMillan Streets.

### Parking Signage

Parking availability in Stratford should be promoted by directional signage in Tyers Street at both its north and south approaches to the town. This would also assist in reducing traffic/parking conflicts to the town centre during very busy shopping periods.

It is also important to establish long term bus/coach parking facilities on the west side of Hobson Street, allowing drop-off facilities for schools and tourist traffic. This can also include trailer and caravan parking, which can be located to the north side of Memorial Park.

### 4.3.2 Pedestrian Amenity

Pedestrian amenity is a major concern, and greatly affects the shopping area of the Town Centre. The most identifiable pedestrian access routes within the shopping area are:



Figure 12 - Dawson Street / Tyers Street intersection, a wide streetscape lacking pedestrian safety.

- Crossing Tyers Street to shops on either side of the street, with the greatest activity (peak and out-of-peak season) between Raymond Street and Blackburn Street.
- The remaining pedestrian traffic is along the shopping area footpaths.

A major problem with pedestrian access is the poor road crossings across the busy Tyers Street, pedestrian visibility hindered by the low growing barrier of the central median melaleucas.

Another point is the conflict of service and delivery vehicles which double park during the peak visitor period, causing a traffic hazards as well as reducing parking space.

#### Opportunities

- Provide new footpaths to the shopping area.
- Improve access from the rear of shops to Tyers Street.

- Provide efficient weather protection to the shopping area and bus waiting areas.
- Provide a bicycle/walking track, linking the Avon River to the Stratford shopping area.
- Pedestrian access across the Princes Highway (Tyers Street) needs to be improved to provide links between car parking areas and the shops. This is probably best achieved by kerb extensions between parking areas, reducing the width of road to be crossed and improving visibility of pedestrians for motorists. This could be complemented by a formal pedestrian crossing at the busiest locations.
- Threshold treatments at either end of the town centre should be provided to reduce the speed of traffic entering the retail precinct. This could best be achieved by intersection works at Tyers and Dawson Streets, and by the use of an avenue of trees. Again this may be undertaken at the Avon Bridge entrance.

#### 4.4 A Survey of Public Response

A survey was undertaken to determine how shoppers and visitors, together with local business people in the shopping area, regard the townscape and other specific features of Stratford from a townscape point of view.

The approach in this work has therefore been designed to link people's perceptions about the shopping centre with the related aspects of shopping and visitation patterns in the centre, what they believe is necessary in respect to new retail and other services and, importantly, how they rank townscape improvements with upgrading of retail services and other elements in the commercial centre.

In summary, therefore, the survey program has been designed to identify people's attitudes to the following:

- The level and range of existing retailing and other services provided in the Stratford shopping area;
- The improvements which they see as necessary to the town centre (in shopping, landscape, parking, etc.);

- Their attitudes to potential improvements in particular areas or specific opportunities (such as tree planting, retention of buildings, etc.); and
- Their comments on the appearance and requirements of specific public areas.

#### 4.4.1 Approach

A survey was undertaken:

- a) A collection of completed survey forms of people living within Stratford and visitors.

The surveys were undertaken in February-March 1993.

#### 4.4.2 A Survey On Local Attitudes to the Stratford Shopping Centre Area.

This survey was designed to obtain residents and visitors interests and attitudes towards the shopping area of Stratford.

Findings from a survey of 69 No. residents / visitors collected in the shopping area are summarised below :

##### 4.4.2.1 *"Resident status"*

The survey indicated that 99% of respondents visiting the shopping area were residents, and 1% being visitors. 46% of those surveyed visited Stratford on a daily basis, 18% on a weekly basis.

##### 4.4.2.2 *"Why do you visit the shopping area"*

71% of respondents are on a shopping trip, with 17% incorporating recreational activities and 7% for transport facilities.

##### 4.4.2.3 *"What facilities do you visit in Stratford"*

The main facilities to dominate are the newsagent (68%), shopping/foodstores (63%), and professional services such as doctor, bank (50%).

The remaining facilities are listed in priority :

Restaurants/takeaway	60%
Hardware	41%
Recreation facilities	36%
Hotel	26%
Post Office	7%
Others	35%

#### 4.4.2.7. "What improvements are necessary"

Interviewees were asked what they thought could be done to make the shopping area a more attractive environment. 35% responded with improvements to shopfronts general appearance, such as signage, painting renovations, and presentation. 17% Of respondents wished for the shopping area to remain as it is. other comments include :

- Provide landscaping - 41%
- Weather protection of shopping area from winds and rain - 23%
- Introduce a theme to the shopping area consistent with the original settlement, heritage. - 23%
- Outdoor cafe areas - 19%

Other improvements and facilities requested:

- Improve pavements for outdoor cafes, avoid obstructions to pedestrian movement;
- Retain "old charm" of Stratford;
- Provide facilities for poetry / music / festivities;
- Improve gardens to main street;
- Provide new park/street seats;
- Remove excessive advertising signs;
- Uniform verandas;
- Slow down traffic through town;
- Remove and replace median strip trees;
- Extend variety of shops; and
- More parking close to shops.

#### 4.4.2.8 "Are you satisfied with facilities available in the shopping area".

Although in the previous survey question improvements were a high priority, 36% of respondents were satisfied with existing facilities, and 22% were dissatisfied.

Suggestions for new facilities included :

- Chemist - 47%
- An automatic bankteller machine longer banking hours(no lunch closure) 5%
- other shops -TAB/sports shop/shoes/health food/drop in centre for teenagers/florist
- A larger Supermarket
- heated swimming pool

4.4.2.9 "What improvements are desirable for the future"

Interviewees were asked to rank their attitude to a list of improvements or aspects of the shopping area, which will improve its appearance and function.

- 39% - Retention and renovation of older buildings
- 39% - Provide new seats, signs, bins, lighting
- 32% - Improve shopping area, streetscape, paving
- 34% - Provide tree avenues to town entries/Tyers St
- 25% - Provide a central information centre
- 23% - New building development
- 16% - Greater recreation facilities.

Overall, great emphasis was placed on major improvements for the townscape of the shopping area, although the response for new development to reflect the character of the Stratford was low.

Other suggestions for improvements include:

- Centralised notice board;
- Improve accessibility of Knob Reserve;
- Improve main street footpath;
- Apex Park to have adventure playground;
- Need for basketball/roller blades;
- Remove median trees to Tyers St;
- More BBQ's required in town/reserves;
- Provide more recreational facilities associated with the river;
- Walking tracks along river and into town;
- Information signage on shops;
- Improve industrial estate appearance;
- Bus stops/shelters for school kid excursions;
- Station fitness track on one of the reserves;
- Golf course;
- More children's facilities; and
- Clean up the River.

Whilst shopping provision, is overall, a dominant activity in the shopping area, it is significant to note that respondents saw a great need to upgrade and enhance the physical and amenity aspects of Stratford.

4.4.2.10 Traffic and Parking

"Is traffic a problem?"

Response to this question was relatively low, mainly due to the fluctuating population in Stratford during the holiday period. A majority of respondents (56%)

indicated that traffic was a issue, and conflicts, during the peak season, can be resolved by safety traffic policies. 22% interviewed felt that pedestrian safety was an issue. 16% indicated service vehicles were cause for concern. Problem areas are:

- two road lanes at Bowls club corner (Tyers St);
- access to Princes Highway from Tyers St north; and
- urgent need of pedestrian crossings during peak traffic periods.

Overall, traffic is a great problem, although a 40km speed limit through the shopping area would reduce the impact of traffic, with the incorporation of pedestrian crossings. Service vehicles and coaches could be encouraged to utilise the side streets where designated.

*"Is car parking a problem and what are the main conflict areas?"*

Again, as with traffic, parking problems are associated with the peak holiday period ( although only 13% agreed that parking is a problem and 32% agreed that no problems exist ). A parking management strategy is required, promoting defined parking areas, off street parking to the shopping area, and signage.

Comments include :

- Slow down through traffic;
- Main conflict areas are between the General Store and Newsagent;
- Conflict due to parking in front of shops;
- Main street traffic too close to pedestrian areas;
- Difficult for pedestrians to cross Tyers St;
- Delivery vans obscure sight lines in main st;
- Need more parking for passing trade;
- Provide trailer/caravan parking;
- Dawson Street/Bowling club - temporary parking a traffic hazard; and
- Encourage use of train service/provide directional signage.

#### *4.4.2.11 Summary*

From the survey it may be surmised that improvements to the shopping area are likely to improve the level of patronage of the area by residents as well as visitors, in the future. The survey has also been useful in

Other interests in Stratford include:

- Market;
- Day care centre;
- Library;
- Bowling Club;
- Antique shop; and
- Service station.

The results show that the location of the newsagent and supermarkets are typically the focus for daily and weekly shopping visits for most of the residents and visitors( reaching a peak during the holiday period).

#### *4.4.2.4. "What areas do you utilise in Stratford"*

Again, the survey has indicated the shopping area is a major focus, with a reply of 70%. Parks and leisure facilities attracted 41%, and the Avon River and reserves a figure of 36%.

Other interests indicate the major activities enjoyed by both residents and visitors:

- Scenic values of Stratford with the Avon River;
- Knob reserve;
- Saturday market;
- Hotels/ cinema;
- Mechanics Hall/ Stratford/ Memorial/ Theatre/ Festival; and
- Bowling Club.

#### *4.4.2.5 Suggestions For Improvements*

Respondents surveyed were asked to nominate improvements, new shops or facilities which they would wish to see introduced to Stratford, and they were asked to give examples where possible. This survey question offers the respondents an opportunity to provide a "wish list" of improvements, although the survey has been designed to promote objective issues. In practical terms it provides an indication to the trading community on their current retail activities and services they should further promote for the benefit of the shopping community.

#### *4.4.2.6 " Do you think the shopping area requires improvements"*

Over 35% surveyed indicated improvements were necessary, with 15% rejecting changes.



identifying the importance of townscape improvements, as well as improvements in the retail role of Stratford. It also indicates that there is considerable support within the residential and visitor community for improvements to be made to the shopping area, with a consensus that "the character of Stratford be retained".

A summary of respondents comments is included below, under category headings:

#### STRATFORD'S CHARACTER AND IMAGE

- Retain Rural character, history of the area and the Avon.
- Stratford has a strong community commitment.
- The town is small in scale, friendly. Many of the residents wish the town to remain as it is, and not to radically change its appearance or function.

#### TOWNSCAPE IMPROVEMENTS

- Provide unified shopfront presentation.
- Improve pavements, introduce pedestrian crossings within town centre/shopping area.
- Retain single storey development, no high rise, reduce built development impact.
- Introduce building and signage guidelines.
- Introduce verandas and outdoor eating areas when considering pavement renovations.
- Merrick Street requires clean up.
- Landmark to denote site of former Shakespeare Hotel (for Shakespeare Hotel).
- Improve the appearance of Mechanics Hall.
- SEC cables to be bundled or undergrounded in main street.
- Provide a Chemist shop.

#### TOURISM AND RECREATION ASPECTS

- Improve appearance of river and its facilities.
- Improve information signage to facilities.
- Encourage all year- round tourism strategy.
- Provide open space link/footpaths between Knob reserve and the town centre.
- Improvements should avoid turning Stratford into a tourist museum.

## TRAFFIC / PARKING ISSUES

- Introduce pedestrian crossings, with lights at Post Office.
- Promote efficient signage for short term parking. all off street parking to the rear of Tyers Street and side streets..
- Provide bus and car/trailer parking facilities.
- Highway traffic to be slowed down.
- Restrict right hand turn into Tyers Street from Dawson street, from the Bairnsdale direction. Suggest a roundabout to resolve traffic conflict.
- Provide attractively designed town entrance boards.
- Provide traffic measures or signage to indicate single lane traffic at Tyers/Dawson Street.
- Provide well marked bicycle paths through the town.
- Restrict truck parking at corner of Tyers Street near Dawson Street.

## LANDSCAPING

- Clean up and improve river environment.
- Remove central planting of paperbarks and improve presentation.
- Improve the Memorial Park and facilities.
- Central information board required.
- Provide attractive plantings to the shopping area and indigenous plantings to the river foreshore and environs.
- Provide more decoration to Tyers Street, all year round.
- Provide covered shelters to parks.
- More Council initiated activities to keep town tidy.
- Provide avenue planting to all streets.
- Apex park requires maintenance and drainage improvements.
- Eastern entry to town requires upgrading.
- Knob Reserve requires weed maintenance and information programme on wildlife and endemic vegetation.
- Provide landscape treatment to Merrick Street, with visitor view points of river and distant ranges.

## 5.0 TOWNSCAPE MASTERPLAN

### 5.1 Introduction

The Stratford Townscape Masterplan was developed for the purposes of making recommendations to improve the physical framework and visual presentation of the town centre of Stratford. Parking, pedestrian circulation, paving, public space, built form, signage, facade treatment, public lighting, street furniture and planting, are described in the following text. However prior to this it is important to discuss the more intangible aspects of the township, its character and image.

### 5.2 Township Character

In Section 4.1.2, the existing character of Stratford was described in terms of its natural physical setting, its history, and its built form and settlement pattern. This was given further substance and meaning through questions asked in the retail business and public surveys described in Section 4.4. In these surveys, there is a strong response to the retention of the existing character and improving the physical environment of the town centre.

### 5.3 Masterplan Description

The Masterplan (Plan 2) attempts to consolidate the shopping centre with improved parking, pedestrian circulation and provide stronger links to its natural reserves of parkland and the Avon River.

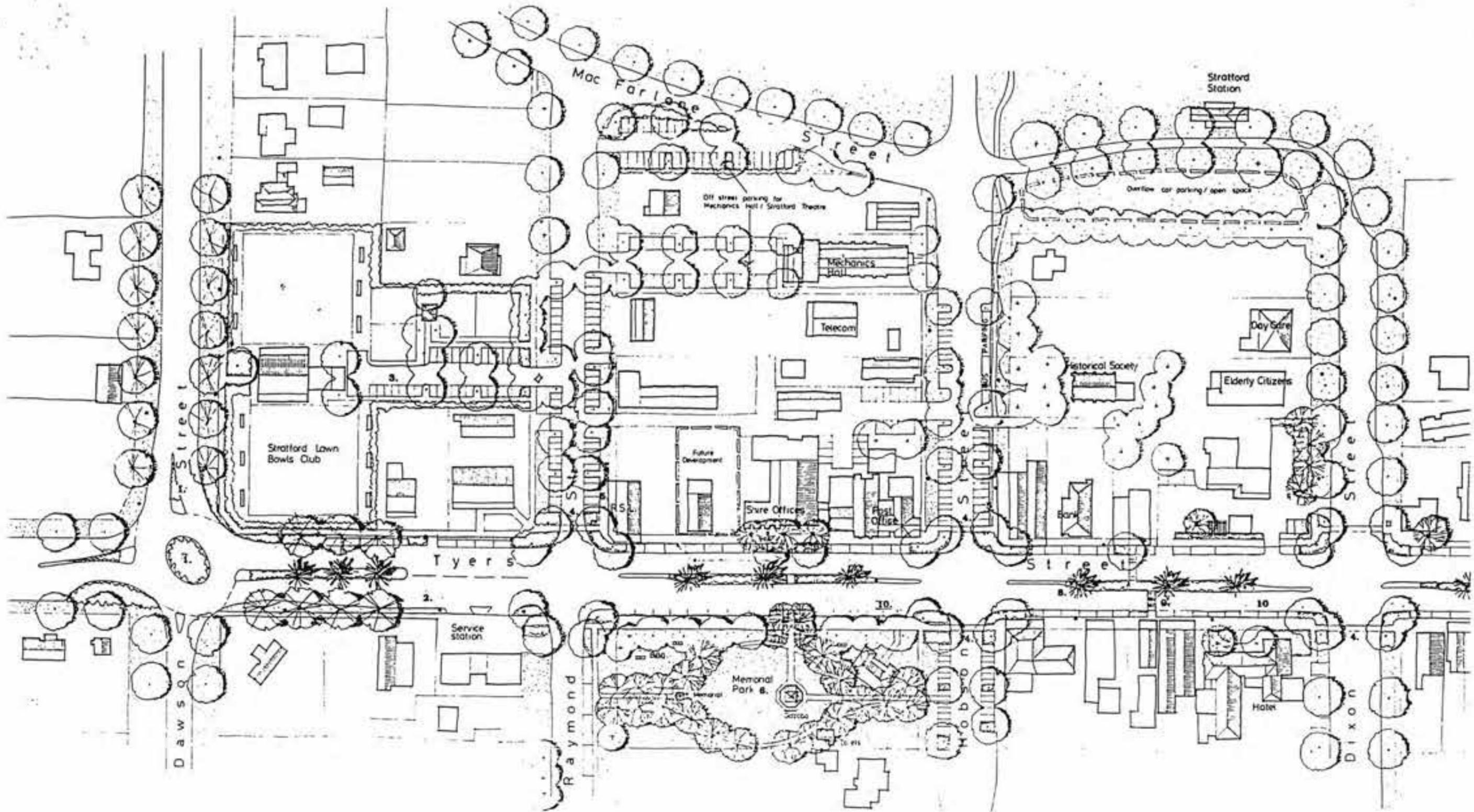
The recommendations of this plan are as follows. (Note: The numbers which appear in brackets thus ( ) refer to the items described on Plan 2).

#### Northern Approaches

This precinct commences at the Water Tower cutting, at the Bairnsdale direction, with the Princes Highway forming a "gateway" into Stratford. The existing industrial site requires improvements to its presentation. A number of improvements are recommended:

- Staged removal of existing large mature pine trees from north side of Princes Highway, in front of the industrial estate. Replace with avenue of *Eucalyptus mullerana* (Yellow Stringybark), to form a "gateway" into Stratford.





1. Create an entry gateway to town centre with avenue trees and provision of a traffic roundabout to the Tyers / Dawson Streets intersection. This will create a safe and more attractive traffic and pedestrian environment.

2. Tyers Street visually narrowed by introducing central median with leisure tree planting and avenue trees to the road edges.

3. Stratford Bowling Club location upgraded to accommodate a new green, recreation facilities and off street parking.

4. Entries to side streets narrowed with corner extensions / entry planting. Landscaped areas to accommodate seating.

5. Memorial Park upgraded to provide leisure lawn with avenue of leisure flowering trees. A central garden provides facility for open air viewing / shelter / bike station (subject to council approval).

6. Shire Office heritage improvements with entry tree planting, paving and seating.

7. Central median with leisure / accent trees and low ground covers.

8. Pedestrian crossing.

9. Extended kerbs with street trees.

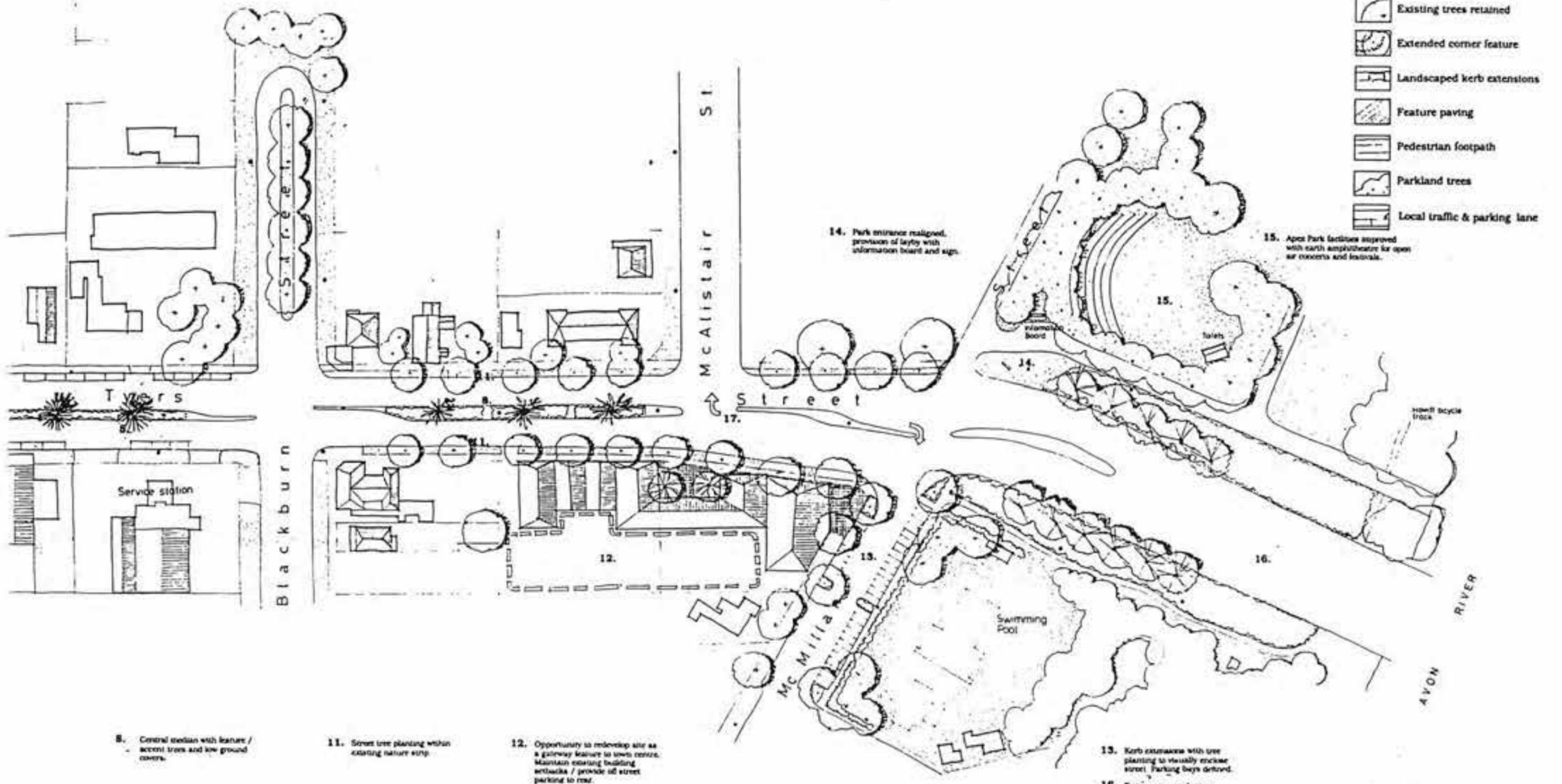


# Stratford Townscape Study



LEGEND:

-  Gateway avenue trees
-  Central median feature trees
-  Main street tree planting
-  Deciduous feature trees
-  Existing trees retained
-  Extended corner feature
-  Landscaped kerb extensions
-  Feature paving
-  Pedestrian footpath
-  Parkland trees
-  Local traffic & parking lane



8. Central median with linear / accent trees and low ground covers.

11. Street tree planting within existing nature strip.

12. Opportunity to redevelop site as a gateway feature to town centre. Maintain existing building artworks / provide off street parking to rear.

14. Park entrance realigned, provision of layby with information board and sign.

15. Apes Park facilities improved with earth amphitheatre for open air concerts and festivals.

13. Kerb extension with tree planting to visually enclose street. Parking bays defined.

16. Entry gateway planting.

17. Proposed right hand turn lane into Mc Alister Street.

# Masterplan

From  
**STRATFORD TOWNSCAPE STUDY**  
 For  
 Shire of Avon  
 Green & Dale Associates,  
 Landscape Architects & Environmental Planners  
 58 Main Street, Plympton 3085. Ph. 417 5323  
 SCALE: 1:500 A1101, 1993 NS 490 1/2 48





- Provide integrated signage to industrial estate, remove excessive/intrusive signage. Council to provide landscape/siting guidelines.
- Stratford town sign to be erected, consistent with townscape theme.
- Tulloch service station to have co-ordinated colour/presentation theme.

#### Dawson Street Precinct

This precinct forms a secondary entrance to the Tyers Street/town centre, but lacks presentation and signage to the centre.

- Remove poor tree species in this precinct, and replant with an avenue of *Eucalyptus maculata* (Spotted Gum) (1).
- SEC lines on northern side of Dawson Street to be bundled, to allow establishment of avenue trees.
- Temporary parking to Bowls Club to be discouraged, with off-street car parking being provided off Raymond Street. Bowls Club main entrance to be encouraged off Raymond Street.
- Creation of a round-a-bout at Dawson Street/Tyers Street intersection to improve traffic circulation.
- Provide landscaping treatment to corner of Bowls Club, and new footpath.

#### Dawson Street to Raymond Street

An important traffic intersection, with a traffic turning conflict at Tyers Street and Dawson Street. Provide a new traffic roundabout to the Dawson Street/Tyers Street intersection. This will visually narrow Tyers Street with a central median, providing a less hazardous traffic zone, slowing traffic, and a safer pedestrian crossing point, at the Dawson Street intersection.

It is essential to create a gateway to the town centre, and a traffic speed control at this point. It is proposed to reduce the wide road pavement by introducing an avenue of *Eucalyptus maculata* (Spotted Gum), with central median planting of feature trees of the same species.

Landscape treatment to be provided to the corners of the intersection, to further reinforce the gateway concept, using low shrubs, and kerb extensions.

Kerb extensions are introduced to all main side streets (4). Raymond Street to provide landscaped pedestrian refuges, incorporating street trees and crossings.

- Raymond Street north, car parking rationalised with one side accommodating 90° angle parking, and the other parallel.
- Kerb extensions are provided for street trees.
- Bus bays are provided adjacent to the RSL building, with the incorporation of a veranda to the north facing wall, to act as a shelter to bus users (5).
- Provide footpath along east side of Tyers Street to Dawson Street.

#### Raymond Street to Hobson Street

Central focus of Stratford, with the attractive Court House, Post Office, and Memorial Park. An opportunity is presented to undertake streetscape improvements to reduce the conflict of busy through traffic, by the introduction of the following:

- Kerb extensions, with landscaping and street trees - *Quercus palustris* (Pin Oak).
- Paperbarks to median to be removed and replaced with a clean stemmed upright street tree - *Eucalyptus maculata* (Spotted Gum). The median would be planted out with groundcover roses (8).
- An informal central pedestrian crossing to be provided, connecting the Shire Offices with Memorial Park.
- It is recommended that the Tyers Street speed limit be reduced to 40 kph to further enhance pedestrian safety.

The Shire Offices frontage is to be improved (7) with feature paving and flowering deciduous trees, incorporating seating. A trellis pergola could be incorporated with the wall of the Shire Offices to enhance its presentation. It is intended this planting will echo the planting theme of Memorial Park. The building could be renovated by rendering orange brick work and painting it, and the wooden trims, the proposed townscape colours.

The RSL building in Raymond Street accommodates intrusive signage on its northern wall, with the '3TR' sign. This detracts from the precinct and should be removed.

Memorial Park will be greatly upgraded to form an attractive park for both residents and visitors (6). Existing planting is to be rationalised, with insignificant species being removed. Feature evergreen tree species will frame the park, a central oval lawn will be formed, incorporating pathways linked to the War Memorial and a new Rotunda. The Rotunda will form a facility for open air concerts, the festival, and shelter. The oval lawn will be framed by an avenue of deciduous trees with strong Autumn colours (Liquidambar).

The park will also incorporate:

- additional barbecues/existing ones relocated;
- an information board;
- infant welfare building landscaped; and
- bicycle storage facility to Hobson Street.

The Stratford Memorial Hall (Mechanics Hall) grounds are to be upgraded, with the inclusion of a car park (36 spaces) and landscaping.

Additional overflow parking is provided on the western side of MacFarlane Street, to serve the Memorial Hall and the Bowls Club.

Hobson Street west and east incorporate corner kerb extensions, with street trees, and 90° angle parking on both sides. Bus/coach parking is provided east of the grocery store, its veranda extended around into Hobson Street, to provide shelter.

### Hobson Street to Dixon Street

The streetscape treatment and kerb extensions are continued on from the last precinct, including the provision of a second road crossing ( features 4/8/9/10).

Improvements are suggested for the former service station site with its take-away cafe and real estate facilities. A pergola is shown to reduce the scale of the existing 'bowser' canopy, with seats and tables. Off-street parking is shown.

The single storey shops are to be improved with theme colour schemes and co-ordinated signage. Streetscape improvements also include removal of existing planter tubs, and replacement with landscaped kerb extensions.

The Stratford Hotel garden frontage to be upgraded with ornamental plantings, reflecting a country garden.

The open space adjacent to Stratford Station is to be improved with parkland planting, with the space serving as a temporary overflow car park as well as passive open space.

### Dixon Street to Blackburn Street

The Tyers Street streetscape theme is continued from the previous precincts. Avenue tree planting is proposed for Dixon and Blackburn (west) Streets. Angophora tree planting is to be repeated on western section of Blackburn Street.

Presentation to National Bank, corner of Dixon Street east, to be improved with kerb extensions and street trees, and feature paving.

Shopfronts to west Tyers Street to be upgraded with veranda, colour schemes and signage improvements. The existing shop, 'Den of Nargun', provides an excellent 'yardstick' of the desired built character. 'Stratford Pizza' requires signage and colour co-ordination.

### Blackburn Street to Avon River

This precinct provides an important 'gateway' approach to the town centre. This area is to continue the streetscape improvements, on Tyers Street, as described in the previous precincts. The only exception is that extended kerblines are not proposed, retaining the open aspect of the streetscape.

Street tree planting is proposed within the existing nature reserves, planting *Quercus palustris* (Pin Oak).

Streetscape improvements will include the following:

- Retain and improve the presentation of the small post support veranda shop, on the corner of Blackburn Street east.
- Signage to take-away shop on the corner of Blackburn Street west, yellow 'Solo' sign is intrusive, and should be reduced in extent.
- A future development opportunity exists on the open block of land, north of McMillan Street, fronting Tyers Street. This site should be planned as a 'gateway' feature to the town centre, maintaining existing building setbacks, retaining the built townscape character and providing a rear off-street car park for visitors and site residents (12).
- A right-hand turn traffic lay-by is proposed into McAlister Street (17).
- McMillan Street west, provision of 90° angle parking, within the Swimming Pool precinct, and extended landscape kerbs (13).

The Avon Bridge gateway is to be improved by providing avenue planting, reinforcing the north Bairnsdale gateway theme, using *Eucalyptus maculata* (Spotted Gum). The roadside embankments, to Apex Park and the Caravan Park, are to be upgraded with weed and rubbish removal, and landscaped (16).

The Apex Park entry is to be realigned, with its entrance closer to Princes Highway, and the provision of an information board and park signage (14). Within Apex Park, the open space below McMillan Street, a grassed amphitheatre is proposed, which can be used for open air festivities, and the Shakespeare Festival (15).

The Avon River foreshore will require an extensive 'clean up' programme, removing litter and exotic weed growth. A walking trail is proposed, linking the Avon with the town centre. The trail can begin at the Knob Reserve, to Apex Park and along McMillan Street to

Merrick Street. At the corner of McMillan and Merrick Streets, a viewing station will provide views to the Avon, the former bridging point, and the distant ranges.

The trail can then follow Merrick Street to Hobson Street, where it joins with the Memorial Park, in the town centre.

### The Railway Reserve

The Railway Reserve, located adjacent to McFarlane Street, is an important open space asset. The precinct can be linked by a pedestrian/bike trail to the Avon River, either by the Hobson/Merrick Street trail, or along McFarlane Street. A further route to be investigated is access between the private residences and the railway land to McMillan Street and Apex Park.

The railway station building, timber weatherboard, forms a focus to the area, and should be retained as such.

The open space to the west and south to be developed as native parkland, with an area reserved for overflow car parking, near Hobson Street.

## 5.4 The Tyers Street Streetscape

### 5.4.1 Streetscape Theme

The improvements to the Tyers Street streetscape are mentioned briefly above. What follows is a more detailed description of this streetscape.

The streetscape of Tyers Street is the central focus of Stratford. The town centre can be strengthened and its character presented through careful streetscape works. The character as defined in Section 5.2 is described in more detail in the following section.

Streetscape works need to be carefully integrated into the existing town fabric. If this does not occur, such works can have the look of add-ons or afterthoughts trying to patch up problems or "tart up" the street.

The proposed design seeks to subtly combine the elements and events which are the essence of Stratford, into a streetscape design which projects the town's image, improves the quality of the street and is carefully integrated into the town's fabric.

Restating briefly Stratford's defined character, it is a:

*A service town to its rural hinterland and it is, historically, an important bridging point on the Avon River. A number of important chapters in history have left their indelible mark on the town. The most significant of these were the gold mining era and the timber industry. A notable asset for Stratford is the Avon River and its parklands. Finally, a significant regional role of Stratford is its festival - The Shakespeare festival.*

This character assessment has guided the design of the streetscape.

Plan 2 shows the proposed streetscape treatment for the Tyers Street, whilst Plans 4 and 5 detail the Civic Precinct and Hobson -Dixon Streets precinct respectively. The significant aspects of the plan are:

- A reconstructed footpath, which is extended to 6.5 metres at crossing points and intersections. Proposed paving is to be precast concrete units as detailed in the guidelines section of this report.

- Parallel parking on the both sides of the street would be maintained. Princes Highway (Tyers Street) still maintains 2 lanes each way of 3.7 metres in width as required by Vic Roads.
- Kerb extensions are created where the footpath is extended to 6.5 metres, at the entries to the side streets and central to the shopping centre street blocks. At these points fixed seating and street trees, with low landscaping is provided.
- Street trees are placed in these kerb extensions areas to provide shade as well as punctuate and emphasize these important nodes. Suitable street tree species are - *Quercus palustris* (Pin Oak). These would provide an attractive addition to the streetscape.
- Paperbarks to median to be removed and replaced with a clean stemmed upright street tree - examples are *Eucalyptus maculata* ( Spotted Gum). The median would be planted out with hardy groundcover roses which will provide colour throughout the year to the streetscape . The two species selected have a formal habit and are evergreen and suited to traffic conditions.
- The form and colour of the proposed street furniture is a reflection of the area's heritage and Stratford's theme as a festival town. The seat, bollard and bin enclosure, are all of robust design. Light standards would be positioned within the pedestrian kerb extensions and Memorial Park for additional illumination (see guidelines section for further detail).
- Weather protection is also shown at two locations in the town centre, adjacent to the RSL building, Raymond Street, and the store on Hobson Street. At both locations a veranda and seating is proposed to the side of the building, which will provide shelter for bus and coach users.
- Memorial Park is to be upgraded to form a central feature within the town centre (see Plan 4). Existing planting to Tyers Street to be replaced with attractive evergreen trees - Smooth Bark Apple Myrtle (*Angophora costata*).

#### 5.4.2 Memorial Park



The existing park pathway to be removed and replaced with a pathway placed around a feature "green". The pathway will be linked to Hobson and Raymond Streets. Flowering deciduous trees will form an avenue / or backdrop to the green, suggested trees are -*Liquidambar styraciflua* (*Liquidambar*). The existing Memorial will be on axis with the pathway to Raymond Street.

Within the central green a bandstand or gazebo is proposed which can be used for open air concerts, or used during the festival period, or provide shelter for park users.

It is also important to upgrade existing facilities in the park, such as providing additional barbeques to the Raymond and Hobson Street entries. At the Hobson Street entry bicycle parking / storage facilities are necessary for visitors enjoying the bike route to the river and parklands.

#### 5.4.3 Built Form

The built form of the Tyers Street streetscape owes much to its role as the Princes Highway. The development of Stratford as a stock route and later a major traffic route spurred the development of the town centre. Today, much of the earlier built character has been lost, such as the Shakespeare Hotel, Stratford's earliest building. The remaining feature buildings are the Court House, Post office, the small shop and veranda to the corner of Blackburn Street, and the later shopfronts with skillion verandas, such as the "Den of Nargun". The remaining buildings range from the 1920's and 30's architectural styles and give the streetscape an integrated character.

We propose to reinforce this character through the adoption of some of the architectural elements which form this style. Plan 3 shows the proposed improvements to the Tyers Street streetscape, in the form of a series of elevations of the existing built facades, which would benefit both the streetscape and the owners of the properties.

#### **5.5 Streetscape Planting**

Where built improvements cannot sufficiently create a strong streetscape character, street tree planting and its related landscape improvements will provide that function. It is important that the streetscape of Stratford does not copy the styles of other regional centres, design should be simple and bold.

Planting within the streetscape is essentially semi-formal utilising local plant species which are tolerant of local conditions, and form a connection to the parklands of the Avon River. Feature trees and low planting in the form of low hedge species and groundcovers will provide the main landscape elements within the extended kerb areas.

Exotic species are also suggested, although in a limited number to highlight areas and provide colour throughout the year, such as:

*Central median planting (groundcovers)*

- Rosa Alba Meidiland var. Meinflopan - white
- Rosa Alba Meidiland var. Meineble - red with white centre.

Tree planting within the streetscape is limited due to the traffic bearing nature of Tyers Street, the limited space and local conditions, and are located within the pedestrian seating areas and street corner/kerb extensions. Tree species will consist :

*Central median tree planting*

- Eucalyptus maculata (Spotted Gum).

*Streetscape Kerb Extensions*

- Quercus palustris (Pin Oak)

One of Stratford's most recognisable assets is the magnificent Avon River and its parklands, which should be retained as they are a significant link to the area's early heritage. But much of this area suffers from weed infestation and erosion, and will require a staged improvement programme to eradicate these problems. Future improvements should build on the indigenous plant species and avoid the introduction of exotic plants.

*River Parklands*

- Eucalyptus tereticornis (Forest Red Gum)
- Eucalyptus viminalis va. racemosa (Manna Gum)
- Eucalyptus nitida (Shining Peppermint)
- Eucalyptus globoidea (White Stringybark)
- Acacia melanoxylon (Blackwood)

The surrounding streets to the shopping centre, such as Raymond, Hobson, Dixon, Blackburn, McAlister and McMillan Streets, can be planted with the following attractive street trees:

*Gateway Avenue planting*

- Eucalyptus maculata (Spotted Gum)

*Residential Streets adjoining town centre*

- Angophora costata (Smooth Bark Myrtle)
- Brachychiton populneum (Kurrajong)
- Pyrus calleryana (Flowering Pear)

*Memorial Park*

- Angophora costata (Smooth Bark Myrtle)
- Liquidambar styraciflua (Liquidambar)



## 6.0 GUIDELINES

The following streetscape guidelines have been prepared to provide direction for property owners and council officers. They reflect the desired theme or character intended for Stratford. It is important that any redevelopment reflect the rhythms and forms of the traditional buildings, and provide a streetscape which residents and visitors can identify with. The streetscape guidelines included in this study cover the following topics:

- Colours.
- Furniture and Fixtures.
- Signage.
- Paving.
- Built Form - Parapet /Roof Line; and
- Industrial.

The philosophy and rationale for the choice of a particular piece of furniture, colour, tree, etc. is given followed by a drawing/description of the object in question. Any technical information is then listed along with where the item can be purchased.

It is important that when detailed design takes place, the co-ordination, organization and general layout of these elements in their streetscape context is carefully considered.

### 6.1 Colours

Stratford, being a product of an English 19th century colonial culture, inherited the rich, heavy colours of this era. This has been integrated with later developments including the bright white trims with brick work of the turn of the century and the explosion of colour, particularly in signage, following developments in paint technology as the century progressed. There is no strong existing colour theme within the built environment which would need to be respected. Instead there is the amalgam of colours and elements and the setting; the broad expansive, flat plain of the Avon River incorporating a few scattered trees. The dominant natural elements are therefore the sky, horizon and the river.

The colour palette chosen for Stratford emphasizes these natural features and the town's cultural background. A deep blue is the primary colour for all the streetscape furniture and fixtures echoing the sky and the water of

the Avon. To provide contrast and warmth, an ochre trim is added; an echo of the Avon River gravel.

Facades and verandas along Tyers Street are proposed to be painted in a series of complimentary and contrasting colours to that of the streetscape works. Four schemes have been devised. They are:

- Blue.
- Terracotta.
- Red; and
- Green.

Note: broadwall colours should be low sheen, timber, high gloss enamel.

#### *Blue*

Veranda trims and posts:	Galaxy (Dulux)
Veranda fascia:	Galaxy (Dulux)
Roof:	Ivory Palace (Dulux)
Broadwall:	Ivory Palace (Dulux)

#### *Terracotta*

Veranda trims and posts:	Terracotta (Wattyl)
Veranda fascia:	Oat (Dulux)
Roof:	Terracotta (Wattyl)
Broadwall:	Orange Scent (Wattyl)

#### *Red*

Veranda trims:	Galaxy (Dulux)
Veranda posts:	Rich Red (Wattyl)
Veranda fascia:	Orange Scent (Wattyl)
Roof:	Rich Red (Wattyl)
Broadwall:	Orange Scent (Wattyl)

#### *Green*

Veranda trims and posts:	Middle Brunswick Green (Wattyl)
Veranda fascia:	New Cream (Wattyl)
Roof:	Rich Red (Wattyl)
Broadwall:	New Cream (Wattyl)

Utilizing these colour schemes will provide a level of continuity which will draw the town together and create a striking image. The exceptions to this are pre 1945 red brick buildings which should be left unpainted (eg: Post Office) and retain their white trims. Post 1945 red and cream brick buildings can be left unpainted or they can be rendered and painted any one of the proposed colour schemes. If left unpainted cream brick buildings should have predominantly white trims with Huckleberry (Dulux) accents. Red brick buildings, should also have predominantly white trims with Sunshower (Dulux) accents.

## 6.2 Furniture and fixtures

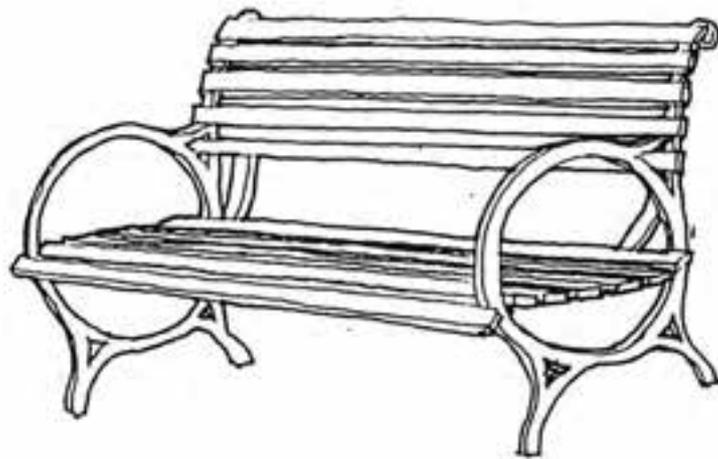
This section covers the following items:

- Seats;
- Bollards;
- Lights;
- Bins;
- Banners;
- Rotunda;
- Park fence; and
- Paving.

We have chosen designs for these items derived from Stratford's English colonial origins, with an emphasis on form and elegant simplicity.

### 6.2.1. Seats

The seats are an interpretation of a nineteenth century American design. They are a simple yet striking design. Metal work would be in the "Galaxy" colour, whilst timber slats would be in the "Oat" colour (see colour section for further explanation). These seats can be obtained from Street & Park Furniture, 30 Chapel Street Norwood, South Australia, 5067. Phone: (08) 362 3634, Fax: (08) 363 1717.



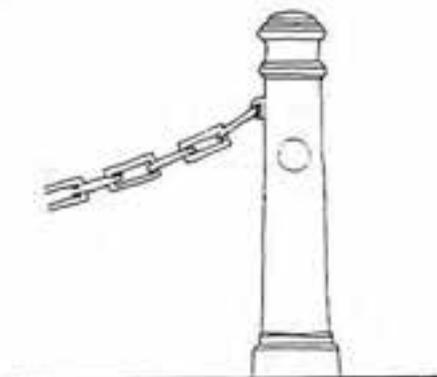
*Seat*

### 6.2.2 Bollards

These are a simple, formal, robust design also from the nineteenth century. Colour would be the "Galaxy" with the protruding bands painted in "Oat". Provision is made to include a cast of Stratford's Coat of Arms on the bollard. It is constructed from cast iron and is also available from Street & Park Furniture.



*Bollard*

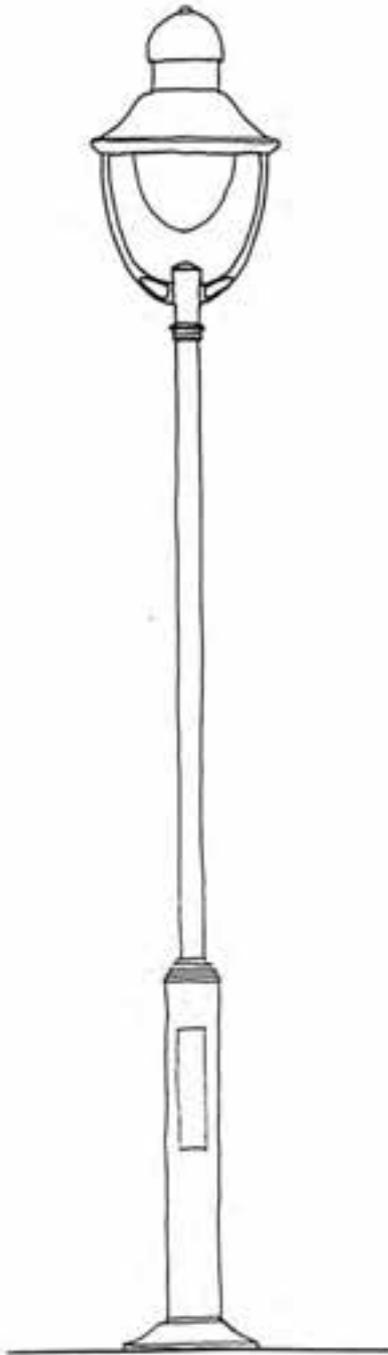


*Bollard and chain*



### 6.2.3 Lights

A traditional design is also chosen for feature lighting in parks, pedestrian focus areas, side streets, etc. It is constructed from polycarbonate, aluminium, diecast aluminium and stainless steel. They would be painted the "Galaxy" colour and mounted on three or four metre poles. These can be obtained through Hunter Douglas Architectural Products, 142-144 Albert Road, South Melbourne, Vic. 3205. Phone: (03) 699 5322, Fax: (03) 699 5411; or a similar version from Simplex Lighting, 18 Apollo Court, Blackburn, Vic. 3130. Phone: (03) 894 1773, Fax: 894 1754.



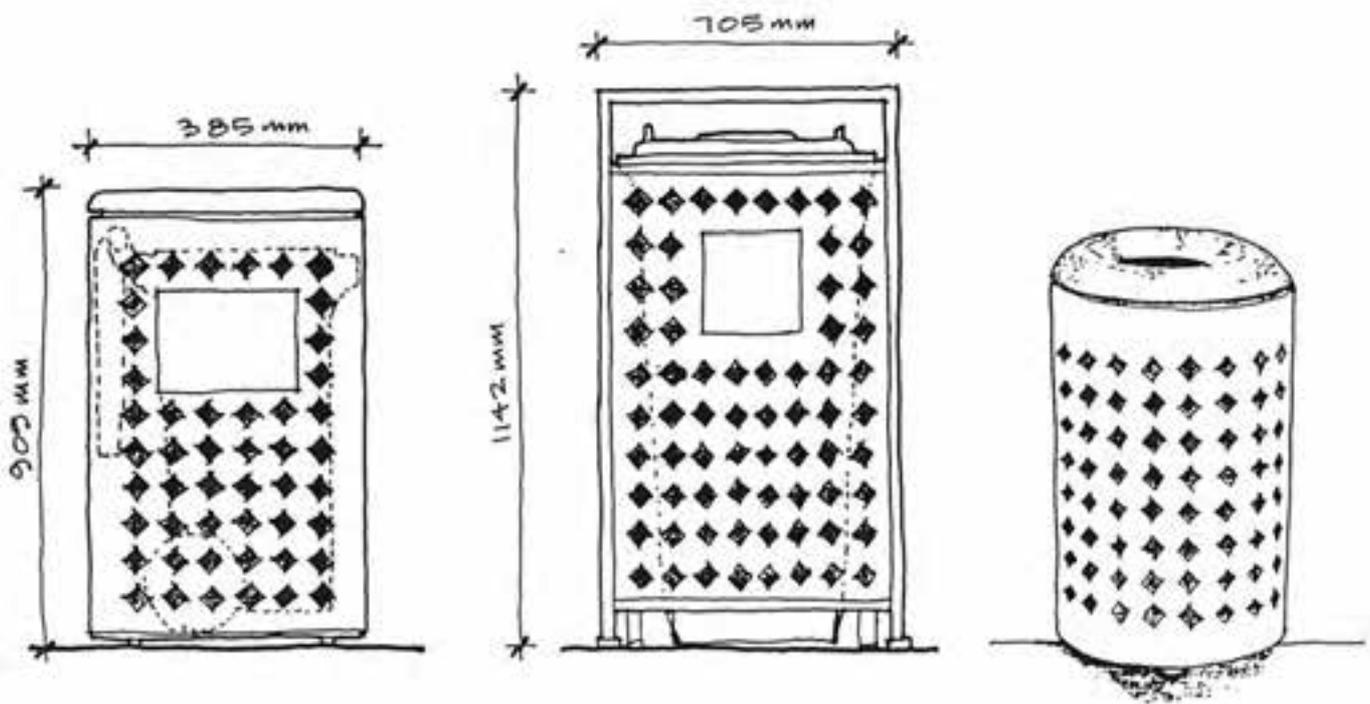
*Proposed Light Standard and fitting*

## 6.2.4 Bins

Two types are proposed:

- Removable liners and;
- wheel-in enclosures

Both forms utilize designs refined by the Melbourne City Council. They are robust, simple and the most effective of their respective types. The removable liner and 80 litre enclosure, incorporate stainless steel or polished aluminum tops; all are lockable. Colour of all types would be "Galaxy".



*80 litre enclosure (Street)*

*240 Litre enclosure (Park)*

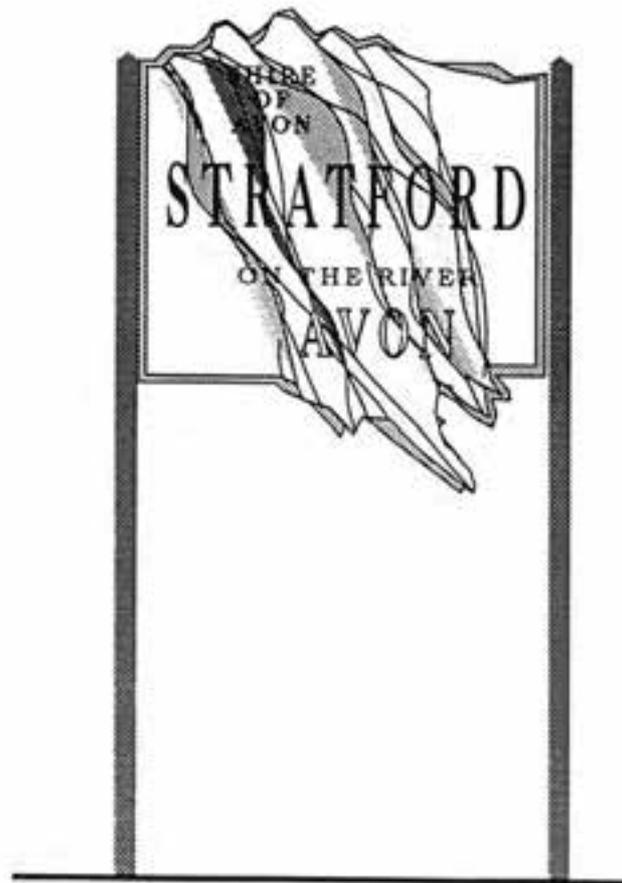
*Removable Liner (Street)*

6.2.5 Banners and Town  
Entry Signage

The Town Entry Sign and Banner design derives its inspiration from the Avon River, Stratford's original raison d'être. The "Galaxy" signifies the colour of the water and the "Oat", that of the gravel and sands of the river bed. The banners can be obtained from any number of flag manufacturers, whilst the town entry sign can be built and painted by a qualified sign writer.



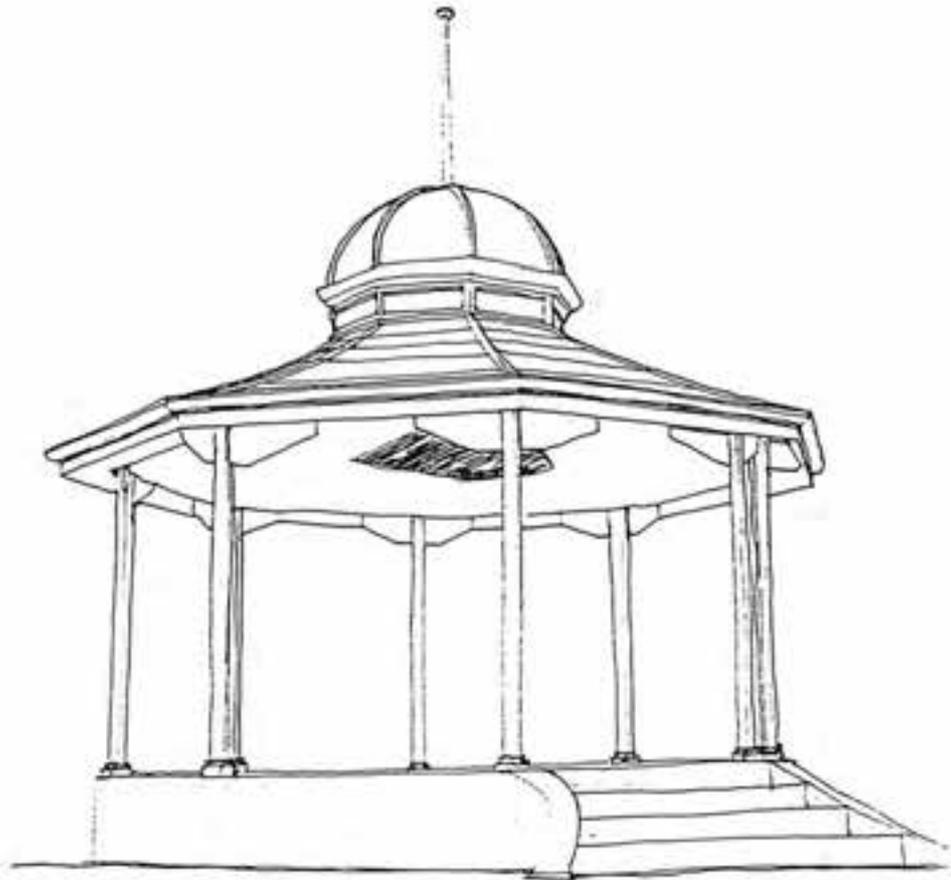
*Banner*



*Town entry sign*

### 6.2.6 Rotunda

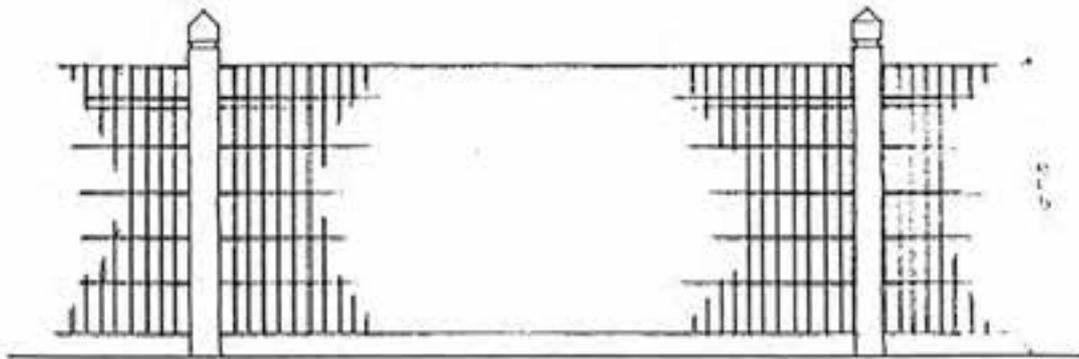
A simple, traditional rotunda design with a minimum of superfluous decorative effects, has been chosen as the most appropriate for Stratford. It is proposed to sit on a brick base raised 600mm above the surrounding ground. Bricks would be Nubrick "Sunset Red" (or its equivalent), with a bullnose topcourse (including stringers). Post bases would be painted in the "Oat"; posts and gutter in "Galaxy" and finally, the roof in "Oat". The rotunda, "Parkville" style, can be purchased through Rotundas Australia, 8 Hawkins Avenue, Box Hill North, 3129. Phone: (03) 899 8228. Fax: (03) 899 8229.



*Rotunda*

### 6.2.7 Park and Pedestrian Refuge Fence

The proposed fence for these two uses, utilizes a simple square post with minor decorative effects and a secure galvanized steel mesh fence. This provides park users with an attractive and effective fence. The posts would be in "Galaxy" with a band of "Oat" in the routed groove. The steel mesh would be powder coated "Primrose".



*Fence*

### 6.2.8 Paving

The Avon River has been, and still is, a source of aggregates used in exposed concrete blocks for walls and paving. The warm creams of this stone are attractive in colour, whilst providing a good, non-slip surface. We propose to use a series of pre-cast concrete pavers for pedestrian paving, arranged in a traditional decorative pattern.

In order to integrate this proposed paving with the insitu concrete, we have chosen a complimentary colour scheme for the pavers. It consists of pale yellow ochre and terracotta, all with exposed Avon river pebbles to the surface except for the terracotta colour.

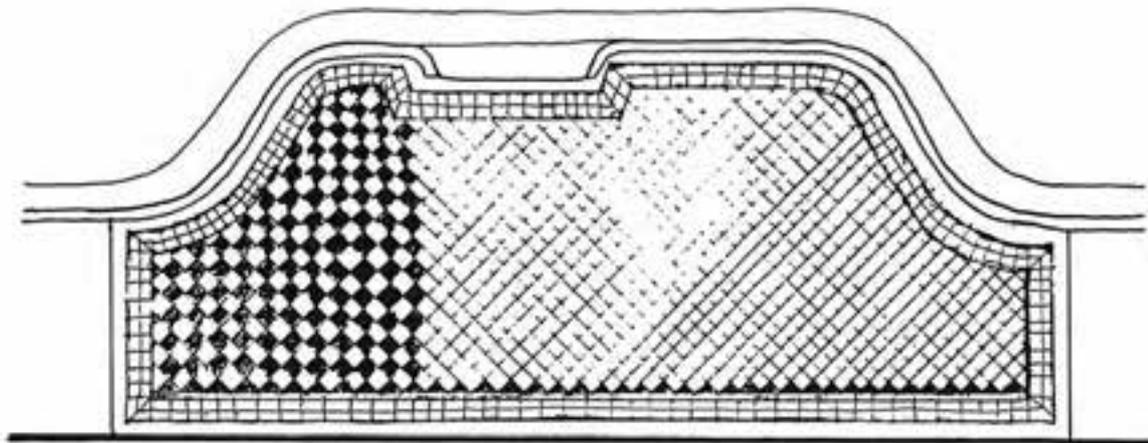
The drawing below uses two sizes:

- 230 x 220 x 50mm double block surround;
- 230 x 220 x 50mm diamond pattern infill.

These sizes are those produced by a local manufacturer.

The surround would be in the terracotta as would one of the diamond infill colours. The infill diamond pattern incorporates the terracotta already mentioned with the pale yellow ochre described above.

Finally, it is of crucial importance that the quality of finish is high. To ensure this, a fine textured, non-slip finish to all surfaces; a clean unrumpled and non-bevelled finish to all edges; and consistent and clear colouring are essential to produce an attractive and high quality paved area.



### *Paving*

## 6.3 Advertising

The intent of this section is to encourage creativity in advertising and integration with the architectural design of buildings and the townscape. The idea of any control or restriction on advertising can be a difficult idea for retailers to accept. Retailers see it as their right to advertise their products in the most effective way. This leads to conflict in the visual character of towns. Stratford is for the most part, uncluttered and far less aggressive than the majority of urban districts in Victoria. This existing situation (a cross section of which is shown of the following page) is a good base on which to build a town of high visual quality.



*One commercial brand dominates the building, occurring five times in various sizes and forms. Additional chaotic images are scattered over the building. Shop name unseen. No co-ordination of signage or colours with that of the building (which is good).*



*Good example of co-ordination of signage with architecture and colours. Shop name prominent and most dominant signage.*



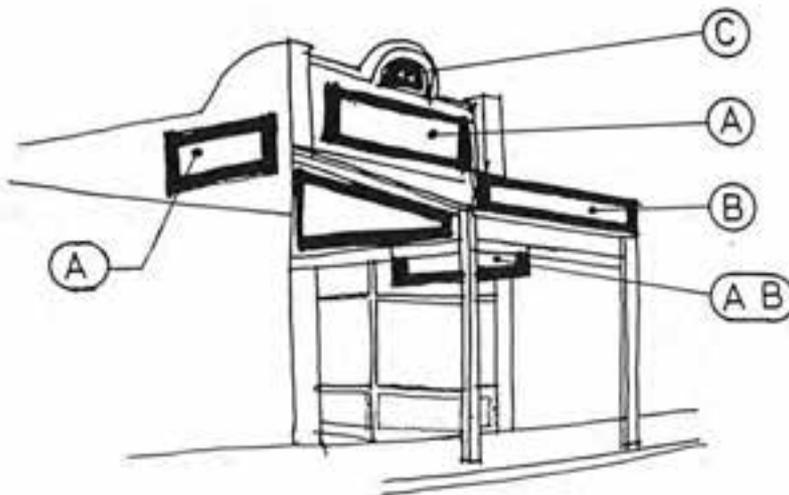
*Excellent example of signage co-ordinated with building form, style and colours.*

*Examples of existing signage in the Town of Stratford.*

As a general rule any signage should identify the shop name only and give only minimal information about the nature of the business carried out or service offered. Commercial brands can be displayed, however they should be subordinate to the store name. The use of a large crest, logo or other corporate insignia is to be discouraged as it competes with the overall presentation of the town.

In general terms the **hierarchy** of signage should be as follows:

- (a) *Below veranda or canopy level:*  
Detailed, specific information about the goods and/or services offered. This signage should be organised as part of the shopfront design.
- (b) *Canopy Level:*  
Information about name and nature of business, eg: "Fresh Meat," "Small Goods", "Newspapers", "Prescriptions", "Telephone Number".
- (c) *Above veranda or canopy level:*  
Basic business identification, eg: "Butcher", "Newsagent", Pharmacy".



Location Hierarchy of Signs:

- A: *Primary location (business name).*
- B: *Secondary (nature of business, eg: Baker).*
- C: *Tertiary (additional information); and*



*Signage should:*

- Incorporate structural supports which are either concealed from public view, or of an attractive design to enhance public presentation.
- Be co-ordinated with and complimentary to signs for tenements of the same building.
- Not dominate or obscure other signs or result in visual clutter.
- Be constructed of durable materials and maintained in good condition.
- Not emit excessive glare or reflection from internal or external illumination.
- Comply to safety standards for unimpaired vision from vehicles or pedestrians at potentially hazardous areas; and
- not be confused with, or reduce the effectiveness of traffic control devices.

#### 6.4 Design Types

The minimum standard for a sign is one with painted letters on a painted background panel. The panel should be neatly trimmed and detailed to compliment the shopfront presentation. To ensure presentation is of a high quality, the work should be carried out by a qualified and experienced signwriter and be durable enough to maintain an attractive appearance for no less than five years.

Signs which detract from the town's presentation are:

- Strobe or activating lighting.
- Poorly conceived moving or rotating signs.
- Non-durable cardboard or foam lettering.
- Boxed or cabinet type, except where totally recessed.
- Noise emitting signs.
- Luminous, vacuum formed, plastic letter signs.
- Signs painted directly on a shop front signband.
- Those employing unedged or uncapped plastic letters or letters with no returns and exposed fastening.
- Signs using flashing lights of any kind.
- Canvas or other fabric sign panels fixed to or suspended from facade or veranda/awning structure.
- Sandwich board signs which may obstruct pedestrian movement.

- Festoon lighting; and
- multi-exposed luminaires to signs or shopfront borders.

It is important that the setting and how a sign is to be viewed are taken into account. One which is freestanding and viewed square-on should be scaled differently to a sign on a shop which is viewed from an oblique angle.

It is also of prime concern, that any proposed sign, advertisement, notice or other poster or lettering, be given council approval prior to its erection. Proliferation of unapproved advertising can quickly bring the quality of the visual environment down.

#### 6.4.1 Below Veranda Level Signs

These include signs hung from verandas and those often painted or fixed to the transom above door level. The latter should generally be discouraged, whilst those hung from verandas should:

- Be of a small scale (maximum of 1.5m<sup>2</sup>);
- Not be internally lit.
- Be visually interesting.
- Utilize the colours specified in the proposed town colour scheme (see section on this).
- Have a minimum clearance to the pavement of 2.7 metres.
- Have general dimensions of 500mm x 2500mm; and
- be sensitively lit, if lighting is required, utilizing spot lamps (low voltage quartz halogen are preferred).

#### 6.4.2 Above Veranda Level Signs

These include all signage above the level of the veranda. The objective is to provide guidelines which enhance the town's image and provide a simple uncluttered message. These are often the most visible of all types of signage within a town.

The following general rules are proposed:

- Signs should not project above parapet level.
- Should be of a visually interesting design integrated into the architecture of the building and complimentary to the townscape.

- They should have a minimum margin of 300mm; and
- should utilize the colours specified in the colour scheme section.

#### 6.4.3 Wall Signs

These can vary from small and compact to large and overwhelming. The following general rules should apply to their size, style and location. They should:

- Not dominate the building; and
- not be of such a size that they become the prime focus.

### **6.5 Built Form**

The issue discussed in this section relates to the outward appearance of Tyers Street, the main street of Stratford. In general terms the street is a mix of building styles which date back to late last century. Buildings have been spasmodically constructed since this initial development phase, and this has created a patchwork of architectural styles. The dominant form is single storey with or without parapet and veranda.

The streetscape in its present configuration, fails to present a consistent and legible theme. The buildings from pre World War II are the most successful in that they provide enclosure to the streetscape, detail and consistency of form and materials. Those from post 1945 failed to respect these positive precedents and, therefore, weaken the town's presentation. It is proposed here to base future development and rectification of existing development on the effective precedents established pre 1939.

#### *Essential Elements*

The architectural elements which constitute the most effective form of development for Stratford's town image include:

- a simple post support veranda;
- a generous parapet;
- a central or side splayed entry; and
- a simplicity of detailing and materials.

Each of these is discussed below

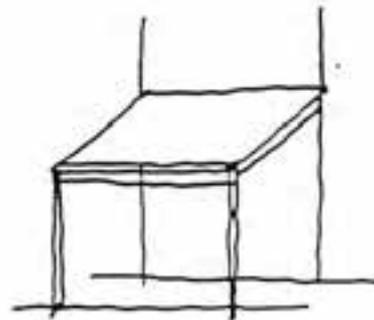
### 6.5.1 Verandas

There is a mix of cantilever and post support verandas within Stratford. The cantilever design is a comparatively modern intrusion (1950's generally) and fails for two reasons:

- Its complete lack of architectural interest; and
- its inability to integrate the streetscape.

The post support style however, fulfils both these objectives and in its modern form can be either semi or totally supported without the posts, ie: in the event of a vehicle hitting a post, the veranda will not collapse.

Our proposal is, therefore, that the verandas of Stratford's Tyers Street should be a post support type of a form similar to that shown below. This "skillion" veranda is simple and cost effective to construct. Due to its inherent simplicity, it can be grafted onto buildings of various eras and styles without causing design conflicts.



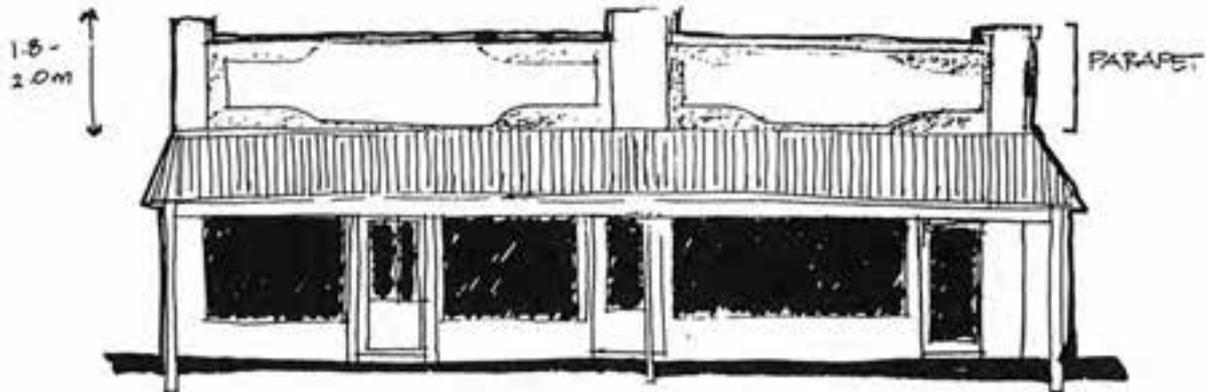
*Straight, pitched, forward sloping skillion veranda at 10° - 20°*

It is important to note that certain buildings were designed without a veranda and have a design integrity which should be left as is. Most noteworthy examples are the Post Office and ex-Commonwealth Bank.

### 6.5.2 Parapets

The original function of parapets was to hide the roof structure, particularly the hip roofs, of buildings. As an added benefit they provide excellent advertising space and enclosure to the streetscape. The most successful of these relate to pre 1945 buildings, where articulation and detail to the skyline, wall and piers is evident.

With the introduction of the flat metal deck rooves, their original function is no longer relevant, however, the additional benefits of enclosure and signage area are. It is proposed that all future developments and renovations to existing shopfronts should include a simple parapet of the general form shown below.



*Two examples of simple parapets relevant to Stratford.*



### 6.5.3 Shopfronts

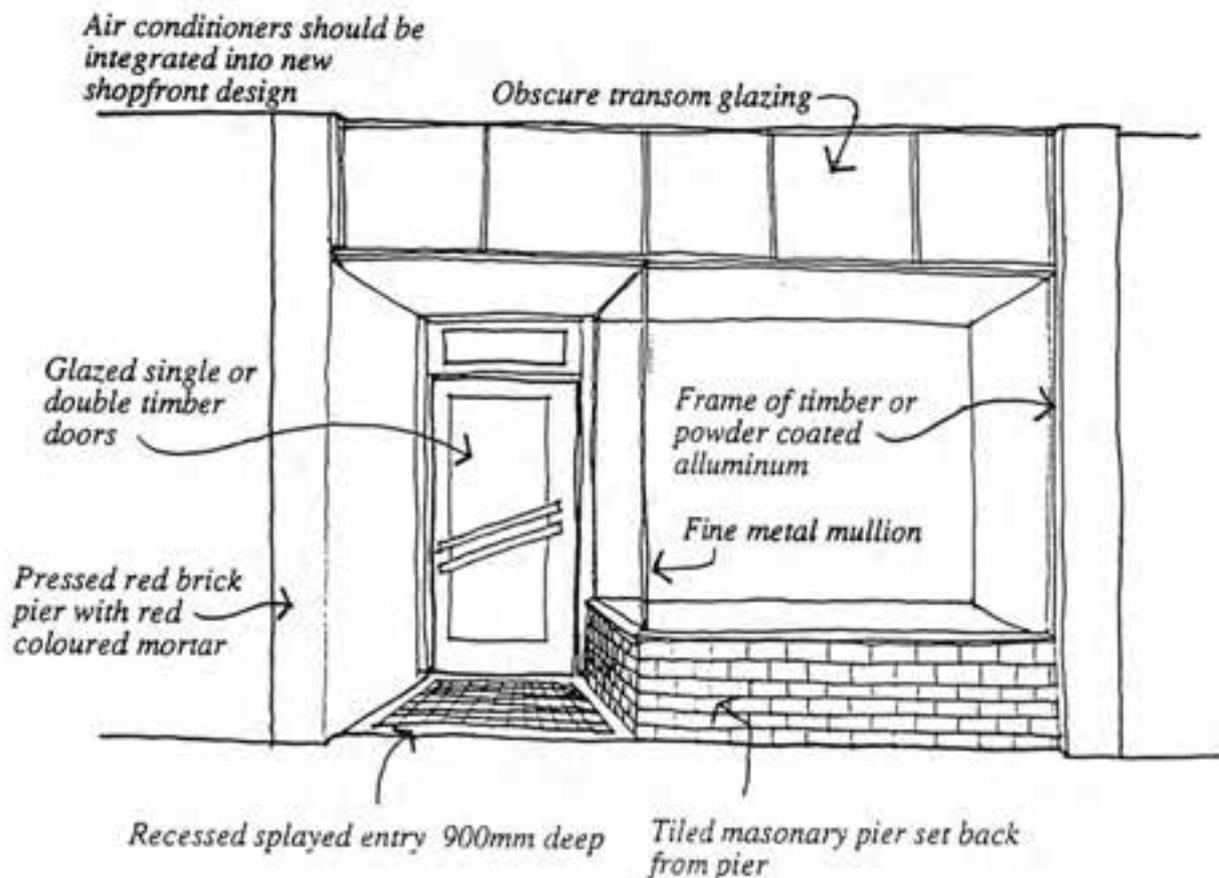
As discussed previously in this section on built form, the most successful buildings in Tyers Street from an Urban Design viewpoint, are those constructed before World War II. They provide:

- A quality display for the goods on offer.
- A frame which compliments the display.
- An interface between the interior and exterior.
- A compliment to the building design; and
- weather protection and security.

The basic elements in their design are:

- One-sided or splayed central entry (approximately 900mm deep).
- Glazed single or double doors.
- Obscure transom glazing.
- Tiled piers and stallboards; and
- frame of timber or metal with fine mullions.

This design is simple and, using modern materials such as powder coated aluminium, quick and relatively cheap to construct.



*Proposed typical single shop front for Stratford.*

#### 6.5.4 Materials

Timber, brick, fine render corrugated iron and glass are the traditional construction materials most commonly used in Stratford. To this concrete, aluminium, "Hardiboard" and corrugated colourbond and zincalume steel have now been incorporated. The more recent introductions need to be handled carefully to enable a

measure of integration with those from earlier eras. Essential in this match, is the incorporation of the tactile quality inherent in traditional materials. Smooth pre-cast concrete, steel and plain aluminium whilst cheap, efficient and durable, stand in stark contrast to pressed bricks, hand render and timber. Relief needs to be given to the concrete slabs, and aluminium powder coated to bring warmth and texture to what are essentially, cold, hard, materials. It is proposed, that in order to integrate new and renovated development with the traditional existing built form, that this "tactile" quality be incorporated into the chosen materials. As mentioned above, concrete needs to be given relief and painted in the Stratford colour scheme; aluminium should be powder coated in the trim colours suggested in the colour scheme; and steel sections, if exposed, such as in a post support veranda, detailed carefully to remove the cold, hard appearance.

## 6.6 Industrial

Industrial sites require large, flat areas of land to accommodate their land uses such as factories, storage, truck and car parking and office areas. The resulting development can be bulky, uninteresting and untidy. This is particularly evident in Stratford's industrial zone on the eastern approach to the town.

The existing setback and service lane are a useful building block on which to develop an attractive and efficient industrial zone.

The following improvements are proposed:

- Eventual removal of existing Pine trees and replacement with local Stringybark (eg: *Eucalyptus mullerana*) arranged in an informal manner. These will link the vegetation of the Hill to the east of Stratford, to the proposed avenue of Spotted Gum which begin at the train line;
- Screening of yard areas with indigenous large, medium and small shrubs. This will also create a link with the remnant vegetation on the Hill; and
- Improve the public presentation to create a high profile and impressive, public entry. Extensive use of proposed townscape colours should be made in these areas.

## 7.0 STREETScape COSTINGS

### 7.1 Avon River to East of Blackburn Street

#### *Road Works*

	<i>Cost</i>
• Realignment of Apex Park entry and bitumen:	\$10,000.00
• Realignment of McAlister Street, Tyers Street intersection, median redesign:	\$4,000.00
Subtotal:	<u>\$14,000.00</u>

#### *Signage*

• Build and erect town entry sign:	\$1,500.00
• Build and erect Apex Park information board:	\$1,500.00
Subtotal:	<u>\$3,000.00</u>

#### *Planting*

• Advanced trees at town entry (3.5m ht):	\$2,750.00
• Groundcover planting @ town entry (includes ground preparation, tubestock and mulch):	\$27,540.00
• Street trees (advanced 3.5m ht):	\$4,500.00
Subtotal:	<u>\$34,790.00</u>

### 7.2 Dixon Street to Dawson Street

#### *Road Works*

	<i>Cost</i>
• Round-a-bout (includes approaches, resheeting of bitumen, crossfall alteration, services, round-a-bout and planting):	Funded by Vic Roads
• Kerb extensions at intersections	
- Raymond Street (4 No.):	\$48,000.00
- Hobson Street (4 No.):	\$48,000.00
- Dixon Street (4 No.):	\$48,000.00



- Crossings
  - Dixon Street to Hobson Street block (Note: roadworks and crossing funded by VicRoads. Costs here are for bollards, chains and feature lights): \$9,000.00
  - Memorial Park/Shire offices (includes all works): \$15,000.00

Subtotal: \$168,000.00

*Planting*

- Street trees
  - sides: \$10,750.00
  - central median: \$2,500.00
- Groundcovers to central median: \$7,280.00

Subtotal: \$20,530.00

*Memorial Park*

- Earthworks (sunken lawn): \$4,000.00
- Bitumen Paths: \$11,500.00
- Concrete edge (125mm wide): \$10,800.00
- Park fence: \$4,970.00
- Seats: \$12,000.00
- Barbeques: \$6,000.00
- Rotunda "Parkville": \$12,550.00
- Entry paving: \$1,785.00
- Garden beds: \$6,400.00
- Irrigation (quick coupling valves only): \$5,000.00
- Lighting: \$20,000.00
- Trees (advanced 3.5m ht): \$10,000.00

Subtotal: \$105,005.00

*Infant Welfare Centre*

- Fence: \$1,435.00
- Pathway: \$500.00
- Advanced trees (3.5m ht): \$3,000.00
- Garden beds: \$900.00

Subtotal: \$5,835.00

**TOTAL: \$351,160.00**

## **8.0 APPENDICES**

### **8.1 Bibliography**

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Stratford Historical Society - *Stratford in the 1880's - The Memories of Dr A South*. 1976.

### **8.2 Relevant Groups Consulted**

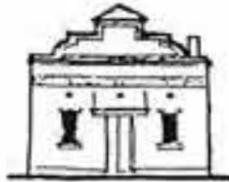
Stratford residents - Townscape/Retail Survey.

Stratford Traders' Association - Townscape/Retail Survey.

Vic Roads Traralgon - Regional Office



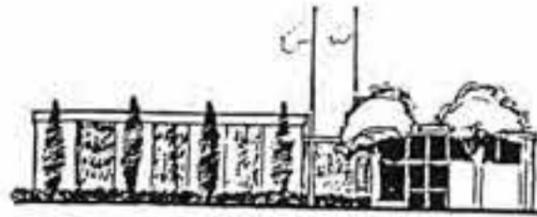
STREET FRONT — FACADE "BUILDING" OF LARGE UPPER STORE BY INTRODUCING COLONNATED PORTICO TO LOWER FLOOR. FINISH MATERIALS TO BE SEEN. ENTRY



REAL BUILDING — FUTURE OFFSHORE SCHOOL TO NORTH WALL. OPPORTUNITY FOR SECTION OF A SMALLER VOLUME TO WITH WALL UNDER ROOF. FINISH MATERIALS TO BE SEEN. ENTRY



OFF BUILDING — FUTURE REFINEMENT OPPORTUNITY FOR NEW BUILDING. SHOULD BE IN BUILDING SET-BACKS OF OFFICE OFFICE & BUILT SCALE



STREET OFFICE — FUTURE PRESENTATION OF OFFICE. HALL FRONT — FINISH FRONT BY ADDING. EMPHASIS TO BE GIVEN TO LIGHT COLOR. BUILDING SURFACES TO BE FINISHED WITH LIGHT COLOR. BUILDING SURFACES TO BE FINISHED WITH LIGHT COLOR. BUILDING SURFACES TO BE FINISHED WITH LIGHT COLOR.



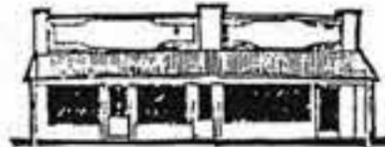
COURTYARD & FRONT OFFICE — FUTURE OFFSHORE SCHOOL. COLOR. FINISH MATERIALS TO BE SEEN. ENTRY



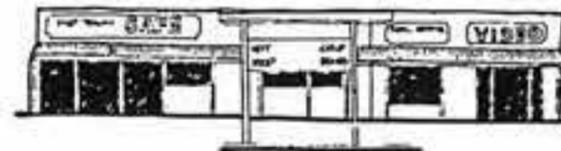
CORNER SHOP — OPPORTUNITY TO CHANGE SHOPFRONT WITH A COLONNATED PORTICO. FINISH MATERIALS TO BE SEEN. ENTRY



STATE BANK — FUTURE OFFSHORE SCHOOL. FINISH MATERIALS TO BE SEEN. ENTRY



SHOP — FUTURE OFFSHORE SCHOOL. FINISH MATERIALS TO BE SEEN. ENTRY



CAFE — FUTURE OFFSHORE SCHOOL. FINISH MATERIALS TO BE SEEN. ENTRY



PUB — FUTURE OFFSHORE SCHOOL. FINISH MATERIALS TO BE SEEN. ENTRY



CORNER VOUCHER — FUTURE OFFSHORE SCHOOL. FINISH MATERIALS TO BE SEEN. ENTRY



MILK BAR — FUTURE OFFSHORE SCHOOL. FINISH MATERIALS TO BE SEEN. ENTRY



DEP OF REPAIRS — FUTURE OFFSHORE SCHOOL. FINISH MATERIALS TO BE SEEN. ENTRY



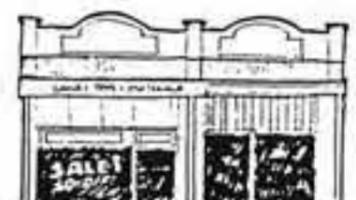
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HOTEL — FUTURE OFFSHORE SCHOOL. FINISH MATERIALS TO BE SEEN. ENTRY



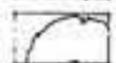
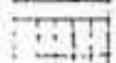
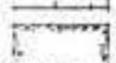
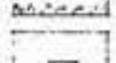
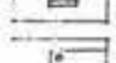
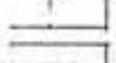
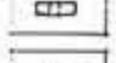
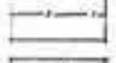
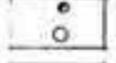
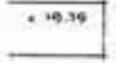
STREET HOTEL — FUTURE OFFSHORE SCHOOL. FINISH MATERIALS TO BE SEEN. ENTRY



FROM STRATFORD TOWNSCAPE STUDY  
SUGGESTED BUILDING IMPROVEMENTS  
Shire of Avon  
Green & Dale Associates  
Landscape Architects & Environmental Planners  
35 Moor Street, Fitzroy 3065, Ph. 417 5322  
NOT TO SCALE February 1993 \$3 486 LC'S



# LEGEND

-  DECIDUOUS FEATURE TREES
-  EVERGREEN TREES
-  FEATURE PAVING
-  LOW SHRUBS & GROUND COVERS
-  SEATING
-  BOLLARDS
-  BARBECUE
-  FENCE (100mm HIGH)
-  PARK LIGHTS
-  PARK POLES
-  SPOT LEVEL
-  HEDGE
-  SCREEN SHRUBS

-  FEATURE PAVING

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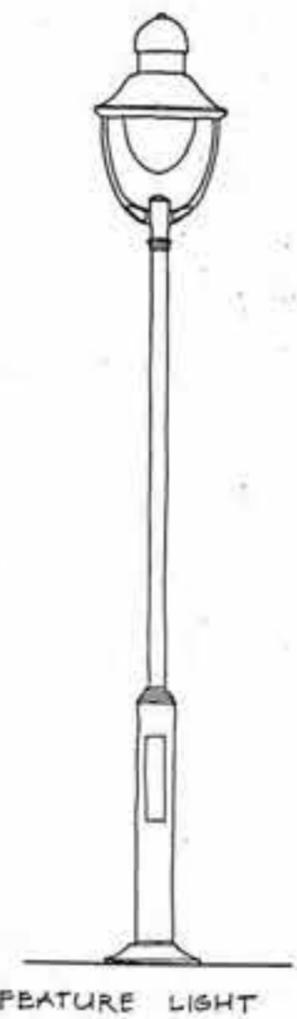
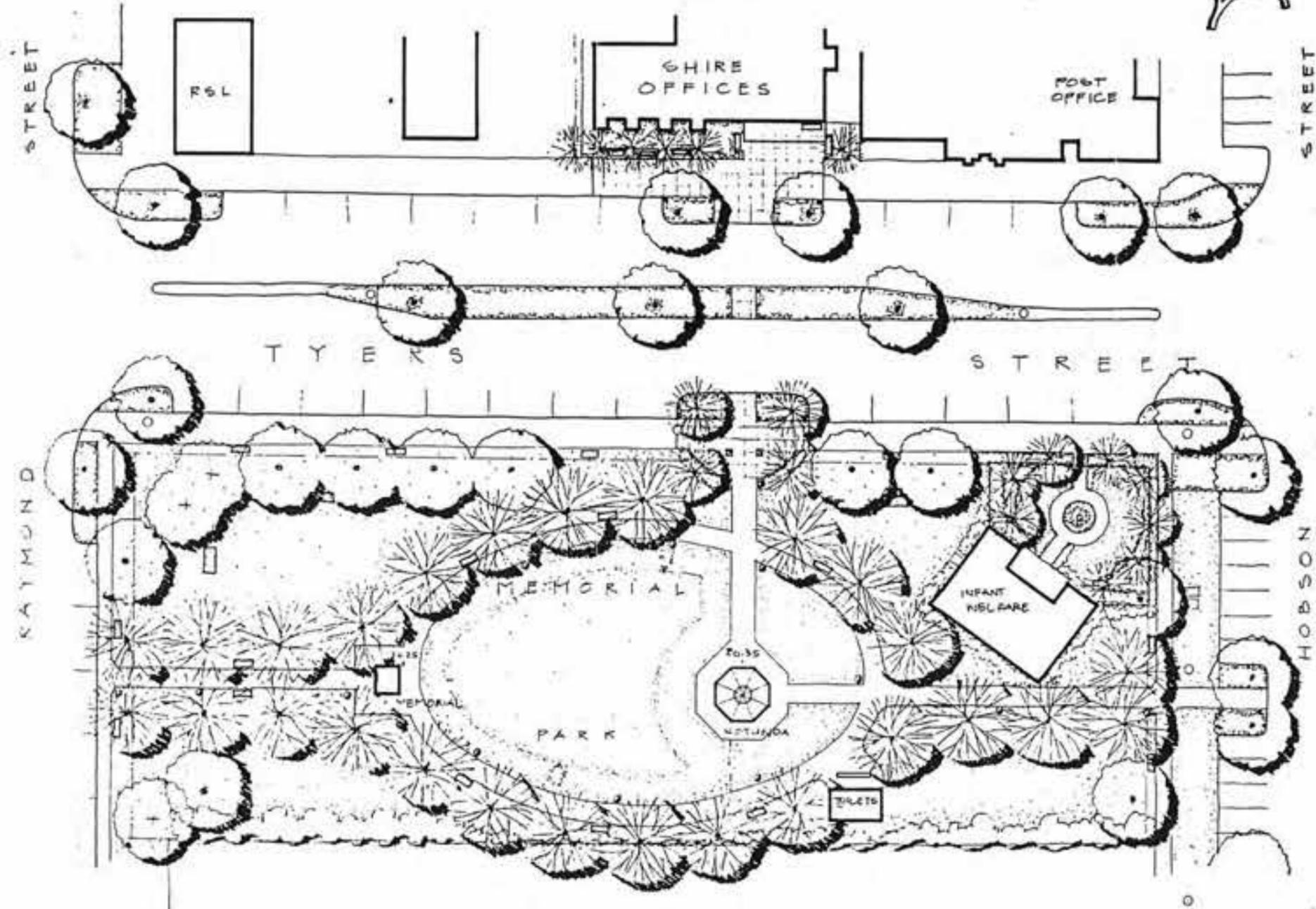
## Stratford Townscape Study

Drawing  
Design Development  
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PRECINCT

Client  
Shire of Avon

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URBAN & RURAL ASSOCIATES  
LANDSCAPE ARCHITECTS & ENVIRONMENTAL PLANNERS  
11 BARRA STREET, STRATFORD, VIC 3177  
TEL: 03 9437 1111 FAX: 03 9437 1112



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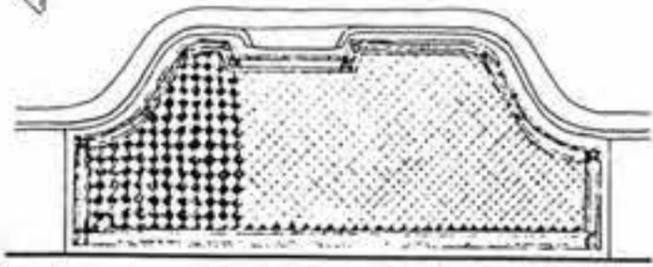
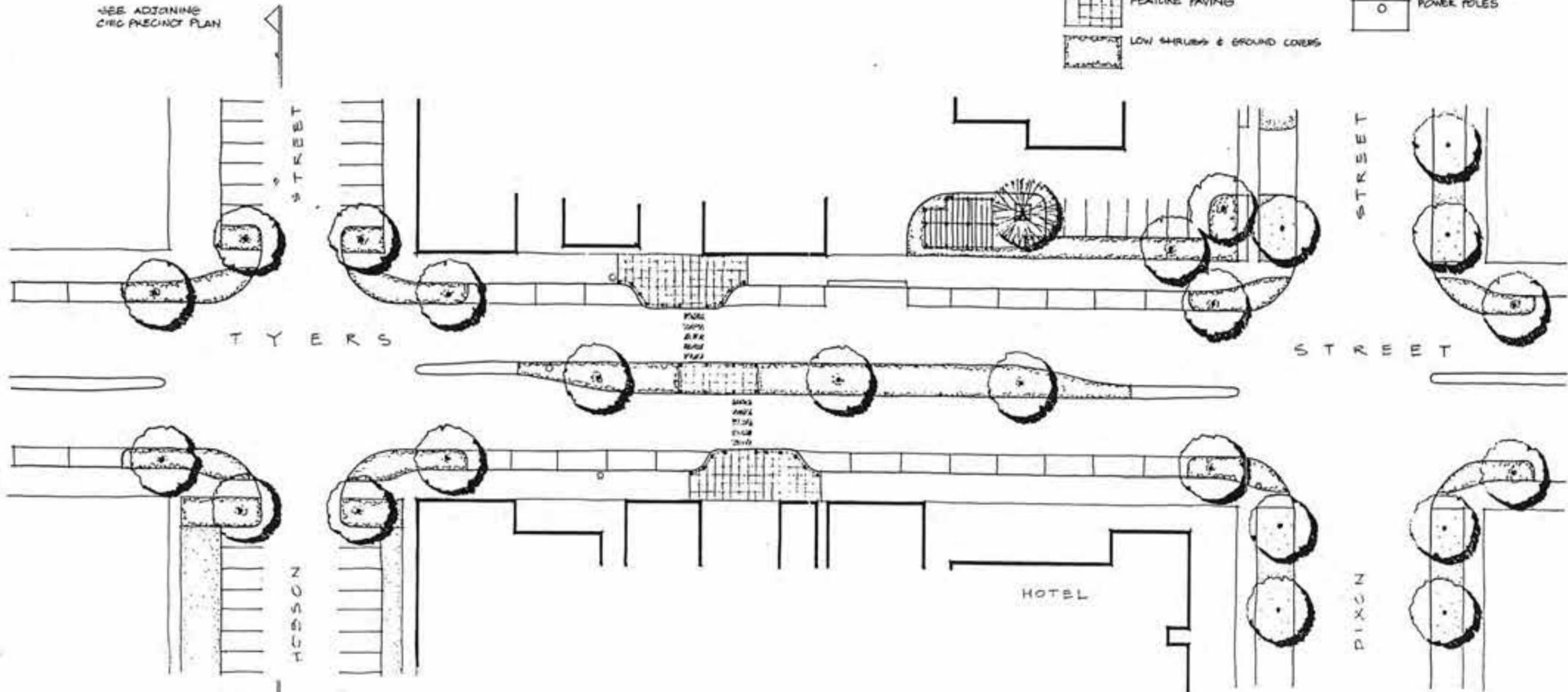


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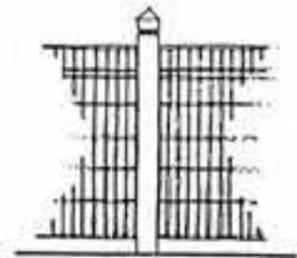
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-  EVERGREEN TREES
-  FEATURE PAVING
-  LOW SHRUBS & GROUND COVERS

-  BOLLARDS & CHAIN
-  MEDIAN REFUGE BENCHES
-  POWER POLES

SEE ADJOINING CIVIC PRECINCT PLAN



FEATURE PAVING



MEDIAN REFUGE



BOLLARD & CHAIN

## Stratford Townscape Study

Design Development  
 JOHNSON STREET TO  
 KON STREET  
 PRECINCT  
 City of Avon

Scale: 1:250, NOVEMBER 1993 93 486  
 A & B Associates  
 ARCHITECTS & ENVIRONMENTAL PLANNERS  
 100/101 GERRARD STREET EAST

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