

Urban Design Guidelines





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## 1 1.1 Background

Wellington Shire Council has commissioned the preparation of the Port Albert and Palmerston Urban Design Guidelines in order to complement the Port Albert Masterplan in setting the future strategic vision for the use and development of land in Port Albert and Palmerston.

The Port Albert Masterplan was prepared by Chris Dance Land Design to provide guidance and direction to the future development of the Port Albert township in both the public and private realm. The Masterplan addressed the following aspects of the township:

- Conservation and interpretation of the historic places and buildings of Port Albert and Palmerston;
- Preservation and presentation of the natural environment (including the coast);
- Streetscape design and function;
- Vehicle and pedestrian circulation; and
- The form and extent of future development.

Whilst the Masterplan provides the strategic framework for the future development of Port Albert, it does not provide urban design guidelines suitable for guiding the recommended built form for the township. It is the Port Albert and Palmerston Urban Design Guidelines which intend to bridge this gap.

## 1.2 Area to which the Guidelines Apply

The Port Albert and Palmerston Urban Design Guidelines apply to all land identified within Figure ## – Port Albert and Palmerston Urban Design Guidelines – Precincts. The guidelines aim to provide a holistic approach to the development (and redevelopment) of Port Albert and Palmerston. Part 4 of this document provides the design objectives and guidelines for the relevant precincts within Port Albert and Palmerston.

## 1.3 Purpose and Objectives of the Guidelines

The Port Albert and Palmerston Urban Design Guidelines aim to provide a sustainable and contemporary design framework that will ensure land use and development in the township:

- Complements the natural and cultural values of the area;
- Complements the maritime heritage focus of the township;
- Accommodates the existing and future needs of the Port Albert population;
- Continues to encourage tourism and investment in tourism; and
- Is consistent with the low density nature of the township.

The guidelines have been prepared to assist the community, developers and the planning authority in designing and assessing the design of new development within Port Albert and Palmerston.



#### 1.4 Policy and Context

The Port Albert and Palmerston Urban Design Guidelines are a policy document, administered by Wellington Shire Council and prepared with the intention of supplementing the Port Albert Masterplan. The Port Albert Masterplan is a reference document within the Wellington Planning Scheme and is the strategic document that has informed the Port Albert Strategy within the Wellington Municipal Strategic Statement.

The Port Albert and Palmerston Urban Design Guidelines is a stand alone policy document, referenced within the Port Albert Masterplan.

As a planning scheme amendment (including ministerial approval) is not required to alter the Guidelines at any point in the future, the flexibility of the Guidelines to adjust to changing demographic or development trends is ensured.

It is envisaged that the Precinct Identification figure within the Urban Design Guidelines will form the Port Albert Strategy Plan within the Wellington Municipal Strategic Statement at Clause 21.06-11. The Port Albert Masterplan should be read as the parent document to these Urban Design Guidelines.

#### 1.5 Public Consultation

To be inserted after consultation

## 2

Port Albert is situated at the mouth of the Albert River in Gippsland, approximately 80km south of Sale and close to the start of the 90 mile beach. Port Albert holds a significant place in Victoria's maritime history, however has been in decline for a number of years.

Recent developments in Port Albert (predominantly residential development) have arguably detracted from the town's historic character and low density nature. The Port Albert community and Wellington Shire Council have an expectation that further development within the township will contribute toward the character of the town rather than detract from it.

The assessment of development applications within Port Albert has also become an increasingly cumbersome task in recent times due to the lack of clarity that exists within existing planning instruments. It is envisaged that the preparation of the Port Albert and Palmerston Urban Design Guidelines will provide a level of certainty for the Port Albert community in relation to built form outcomes in future.

The presence of a diagrammatically based Urban Design Framework has been noticeably lacking for Port Albert. In conjunction with the inherent adversarial nature of land use planning, the assessment of new developments both within and outside of prescribed limits has been based on merits based assessment.

Therefore, for Port Albert to continue to grow sympathetically with its built form and cultural heritage, the overly cumbersome planning framework and lack of design guidance has been removed through the implementation of the Port Albert and Palmerston Urban Design Guidelines and Port Albert Masterplan.



# 3

## 3.1 Overview

The urban design vision for the township of Port Albert have been formed by a synthesis of good urban design principles and a consideration of site features that can be utilised to create a unique sense of place within Port Albert. These aspects have been combined to form an overall vision and set of principles.

The principles of urban design as they apply to both the public and private realm of Port Albert are explained here. The public realm generally refers to the town's streetscape, open spaces, foreshores, tourist precincts and areas of environmental significance.

The private realm generally refers to the areas of the township held in private ownership and is generally limited to residential and commercial areas.

## 3.2 Public Realm

The principal influence of these guidelines as they relate to the public realm is in regard to the interface between the built form and the public areas – streetscape, open space and foreshore areas. Areas of particular focus will include:

- Tarraville Road – Central Arrival Spine
- Wharf Street
- Stockyard Point
- Foreshore open space
- Existing wharf

There is an opportunity to encourage good quality development along Wharf Street overlooking the public foreshore, boat harbour and the bay itself. The effect of continuous two storey development of a similar scale to the existing hotel and the newer buildings will be to reinforce the beauty and attraction of the port area.

Similarly, development along Tarraville Road will enhance the sense of arrival into the township leading from the higher quality streetscape leading up to the entry roundabout.

The redevelopment of the restaurant on the existing wharf will enhance the Stockyard Point area as a tourist destination and a logical built form at the tip of the point.





### 3.3 Private Realm

The principal influence of these guidelines as they relate to the private realm is in regard to built form. The future built form of the township should respond to:

- The character of the harbour and port activity;
- The existing heritage and historical values;
- The coastal environment;
- The need to encourage an increased permanent population;
- The need to increase tourism and longer staying visitors;
- The efficient use of public infrastructure.

The existing buildings within Port Albert are a mixture of 19th century structures, small in number but nevertheless valued, and an eclectic range of housing. Notably, the township has not yet been subject to the 'seachange' pressures affecting some coastal settlements.

Since the main activity of residents and visitors is focussed on boating (fishing in particular) it is not surprising that many of the newer residential structures include facilities/storage for larger ocean going boats.

Five urban precincts have been identified as having distinctive character related to locations or activities (refer Figure ##).

**Precincts 1 and 2** are areas concerned with much of the tourist activity within the township. A relatively higher density should be permitted in these precincts to encourage a critical mass of commercial (tourist related) development in this area.

In addition, the formation of this two storey built form curving along Wharf Street will visually create a sense of public space around the marine structures. The existing hotel sets a good typical standard of architectural form and would be a corner piece for a similarly scaled future development in this precinct.

The new developments along Wharf Street in Precinct 2 similarly provide a guide to the nature of future built form.

**Precinct 3** comprises a well presented group of historically related buildings including the simple but elegant maritime museum. Although the public areas (road and footpaths) lack quality, the buildings are well maintained and future built form should relate closely to this architecture.

Precinct 4 identifies the primary detached residential dwelling areas with an eclectic mix of both permanent and visitor accommodation. Much of this precinct includes undeveloped land.

Precinct 5 is noted as the Central Arrival Spine, the main road into town which is largely under-developed but contains a small core of commercial and retail buildings.

### **3.4 Heritage**

Heritage planning guidelines have been prepared for the Port Albert township to provide a guide for the future conservation and development of heritage protected places in the townships of Port Albert and Palmerston. The Guidelines for the Assessment of Heritage Planning Applications is the document to be referred to for guidance for all planning applications on and adjacent to heritage sites.

# Port Albert Urban Design Guidelines – Precincts



- Precinct Boundaries
- Precinct 1 – Heritage Tourism
- Precinct 2 – Secondary Tourism
- Precinct 3 – (Tarrville Road) Historic
- Precinct 4 – Residential
- Precinct 5 – Central Arrival Spine
- Precinct 6 – Potential Marina

## 4

A number of precincts were identified within the township of Port Albert in the Port Albert Masterplan, prepared by Chris Dance Land Design.

These Urban Design Guidelines have incorporated a number of the precinct boundaries, however some boundaries have been realigned in order to provide a greater level of clarity in relation to desired built form and public realm outcomes for Port Albert and Palmerston.

The precincts (refer Figure ##) as they refer to Port Albert and Palmerston are as follows:



# 4.1

## Precinct 1 – Heritage Tourism

Precinct 1 applies to the area known as Stockyard Point and constitutes an important part of the town's history, as a port area. The precinct contains a tourist focus and is aesthetically pleasing.

### 4.1.1 Design Objectives

- To ensure that the design and finish of new buildings or works complement the built form and character of existing heritage buildings
- To maintain the visual dominance of prominent of existing heritage buildings
- To protect the integrity and distinct nature of the buildings within the Stockyard Point
- To maintain the key sight lines and views to Stockyard Point from the town and the water.
- To design buildings that respond to the scale and built form of existing heritage buildings on the point
- To encourage buildings within the western portion of the precinct to be orientated towards the bay and to provide casual surveillance to the adjoining public area

### 4.1.2 Guidelines

#### Building height

- Buildings should not exceed the height of the Port Albert Hotel (refer Figure ##)

#### Building setbacks

- Buildings and works should have zero lot lines to Wharf Street
- Buildings and works should contain 6m setbacks to North Road

#### Building form

A building should:

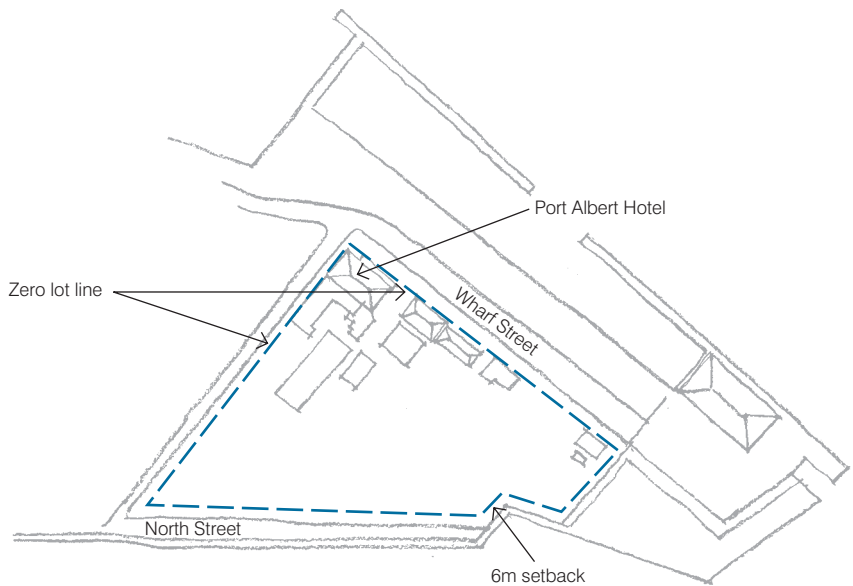
- Use pitched roof forms, either hipped, gabled or a combination of both, pitch between 20 and 40 degrees.
- Use simple verandahs.
- Use rectangular vertical windows where the vertical dimension is greater than the horizontal dimension.
- Use narrow eaves, less than 300mm wide.

#### Car parking

- Off-street car parking, including garages should not dominate street frontages and if proposed should be located at the rear or side of sites

#### Building materials, finishes and colours

- Artificial cladding of buildings and works is discouraged.
- External walls of buildings should be constructed of brick, stone, masonry or concrete. Other materials may be considered if the design objectives are met through a superior proposal.
- Windows at street level should be of clear glazing.
- Exterior colour schemes should be consistent with existing development and maintain consistency and compatibility with adjacent heritage buildings.



- Building height not to exceed height of existing hotel.
- Parking on all lots to be internally and centrally located.
- Signage to existing and new buildings must be in accordance and consistent with "Guidelines for the Assessment of Heritage Planning Applications, Port Albert and District".



# 4.2

## Precinct 2 – Secondary Tourism

Precinct 2 incorporates land immediately to the west of Precinct 1 and bound by Wharf Street to the north, Stockyard Point to the east, North Road and the bay to the south, and the residential precinct to the west; and

Precinct 2 contains a mix of tourist accommodation, tourist retail facilities and residential development. The secondary tourism precinct is largely under-utilised and has the potential to develop significantly stronger links with adjoining precincts, including the central arrival spine, residential precinct and the heritage tourism precinct.

### 4.2.1 Design Objectives

- To ensure that the design and finish of new buildings or works complement the built form and character of existing heritage buildings
- To retain the uniformity of scale throughout the area
- To encourage buildings within the western portion of the precinct to be orientated towards the bay and to provide casual surveillance to the adjoining public area

### 4.2.2 Guidelines

#### Building height

- Buildings should not exceed the height of the Port Albert Hotel (refer Figure ##)

#### Building setbacks

- Setbacks of buildings from street frontages are to be equivalent to adjoining developments
- Buildings and works should contain 6m setbacks to North Road
- Dwelling setbacks should have regard to the setbacks of adjacent buildings

#### Building form

Buildings should:

- Use pitched roof forms, either hipped, gabled or a combination of both, pitch between 20 and 40 degrees.
- Use simple verandahs.
- Use rectangular vertical windows where the vertical dimension is greater than the horizontal dimension.
- Use narrow eaves, less than 300mm wide.

#### Car parking

- Off-street car parking, including garages should not dominate street frontages and if proposed should be located at the rear or side of sites

#### Building materials, finishes and colours

- Artificial cladding of buildings and works is discouraged.
- External walls of buildings should be constructed of brick, stone, masonry or concrete. Other materials may be considered if the design objectives are met through a superior proposal.
- Windows at street level should be of clear glazing.
- Exterior colour schemes should be consistent with existing development and maintain consistency and compatibility with adjacent heritage buildings.





- Building height not to exceed height of existing hotel.
- Parking to be off street.
- Setbacks of all buildings along Wharf Street are to be equivalent to existing building setbacks.
- Setbacks on all other streets to be 6 metres.
- Garages and sheds are to be located to the rear of the front building line.
- Signage to existing and new buildings must be in accordance and consistent with "Guidelines for the Assessment of Heritage Planning Applications, Port Albert and District".



# 4.3

## Precinct 3 – Tarraville Road

Precinct 3 is essentially Tarraville Road and its adjoining premises. Tarraville Road acts as the town's spine and access road to all precincts within the town and to the harbour and wharf.

The Tarraville Road 'spine' provides a mix of commercial/retail functions and consists of high quality streetscape treatment.

### 4.3.1 Design Objectives

- To create a commercial built form along the main entrance into Port Albert which respects the existing built form and image of the commercial area
- To ensure that the design and finish of new buildings or works complements the built form and character of the commercial precinct
- To maintain the visual dominance of prominent of existing heritage buildings
- To promote development that provides a compact and identifiable retail core along significant pedestrian routes.

### 4.3.2 Guidelines

#### Building height

- Buildings should not exceed a height of two storeys

#### Building setbacks

- Buildings and works should have zero lot lines. This may be increased if the setbacks from street frontages are equivalent to adjoining development.

#### Car parking

- Off-street car parking, including garages should not dominate street frontages and if proposed should be located at the rear or side of sites

#### Building materials, finish and colours

- Artificial cladding of buildings and works is discouraged.
- External walls of buildings should be constructed of brick, stone, masonry or concrete. Other materials may be considered if the design objectives are met through a superior proposal.
- Windows at street level should be clear of glazing.
- Exterior colour schemes should be consistent with existing development and maintain consistency and compatibility with adjacent heritage buildings.

#### Weather protection

- New development is encouraged to provide weather protection in the form of a verandah extending over the footpath.



- Building height should not exceed a height of two storeys.
- Buildings along Tarraville Road to have zero lot lines generally. Setbacks may be increased if setbacks from Tarraville Road frontages are equivalent to adjoining development.
- Off-street car parking including garages should be located at the rear or side of sites.
- Building form, materials and colours are to be consistent with existing heritage buildings.



# 4.4

## Precinct 4 – Residential Precinct

The residential precinct comprises a large proportion of the township's area and is located to the east and west of Wharf Street and to the north and south of South Street, in the Palmerston area.

The Port Albert Masterplan notes that the residential precinct contains a mix of both permanent and visitor accommodation at a mix of densities. Large areas of undeveloped land exist within the residential precinct and the suitable development and re-development of many of these areas is a key component of these guidelines.

### 4.4.1 Design Objectives

- To encourage contemporary interpretation of traditional building design within the residential precinct.

### 4.4.2 Guidelines

#### Building height

- Buildings should not exceed a height of two storeys

#### Building setbacks

- Dwelling setbacks should have regard to the setbacks of adjacent buildings

#### Car parking

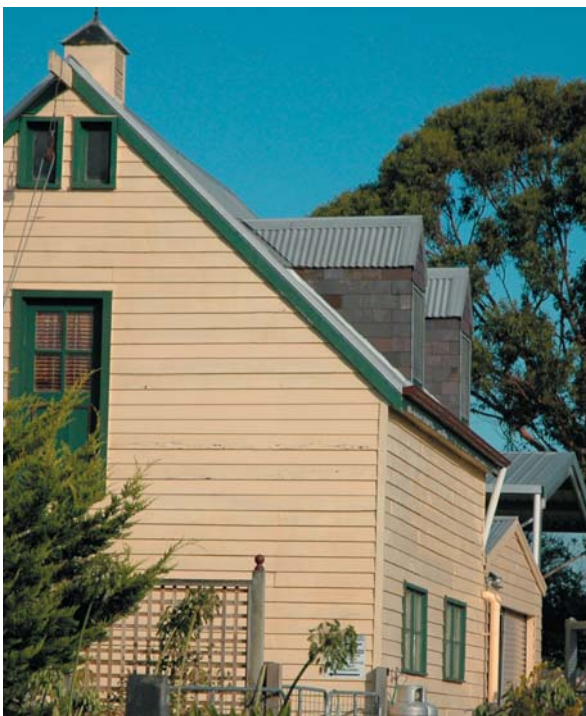
- Off-street car parking, including garages should not dominate street frontages and if proposed should be located at the rear or side of sites

#### Out Buildings

- Garages, sheds and out buildings should not be located in front of the setback of dwellings and preferably should be located and sited to the side or seam of the site.



- Building height not to exceed a height of two storeys.
- Dwelling setbacks should have regard to the setbacks as adjacent buildings.
- Off street car parking, including garages, should not dominate street frontages.



# 4.5

## Precinct 5 – Central Arrival Spine

The central arrival spine is of integral importance to the Port Albert township. The arrival spine leads into the Tarraville Road Historic Precinct, which contains the town's commercial and retail core, albeit it small.

The arrival spine provides the first impression of Port Albert to visitors and reinforces the sense of place to residents of the town. Residential development occupies the majority of Precinct 5 and a reinforcement of streetscape elements is required to accentuate the arrival spine.

### 4.5.1 Design Objectives

- To create a residential built form along the main entrance into Port Albert that contributes towards the built form and image of the urban area.

### 4.5.2 Guidelines

#### Building height

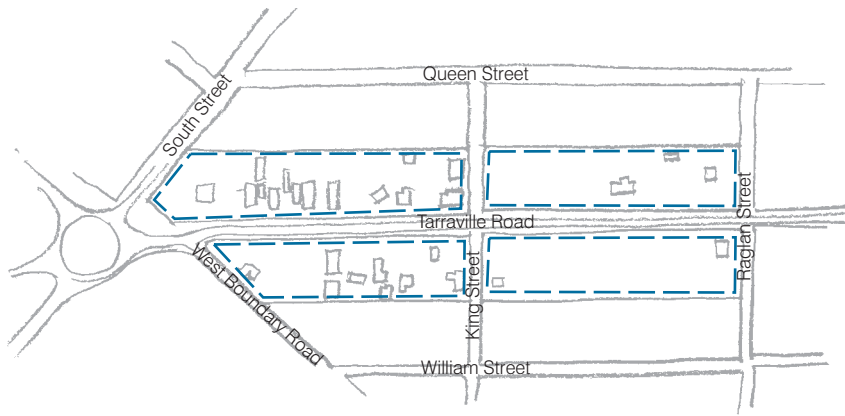
- Buildings should not exceed a height of two storeys

#### Building setbacks

- Setbacks of buildings from street frontages are to be equivalent to adjoining developments

#### Building materials, finish and colours

- Artificial cladding of buildings and works is discouraged.
- External walls of buildings should be constructed of brick, stone, masonry or concrete. Other materials may be considered if the design objectives are met.
- Windows at street level should be of clear glazing.
- Exterior colour schemes should complement existing development and maintain consistency and compatibility with adjacent buildings.



- Building height should not exceed a height of two storeys.
- Setbacks of buildings from street frontages are to be equivalent to adjoining developments.
- Building form, materials and colours should compliment existing development.



**Coomes Consulting Group** Pty Ltd ABN 14 874 072 735  
consult@coomes.com.au · coomes.com.au

24 Albert Road PO Box 305 South Melbourne Victoria 3205 Australia  
T 61 3 9993 7888 F 61 3 9993 7999

144 Welsford Street PO Box 926 Shepparton Victoria 3632 Australia  
T 61 3 5831 4448 F 61 3 5831 4449

Level 4 553 Kiewa Street Albury 2640 NSW PO Box 65 Wodonga Victoria 3689 Australia  
T 61 2 6023 6488 F 61 2 6023 6499

3/23 Gheringhap Street PO Box 449 Geelong Victoria 3220 Australia  
T 61 3 5221 0688 F 61 3 5221 4099