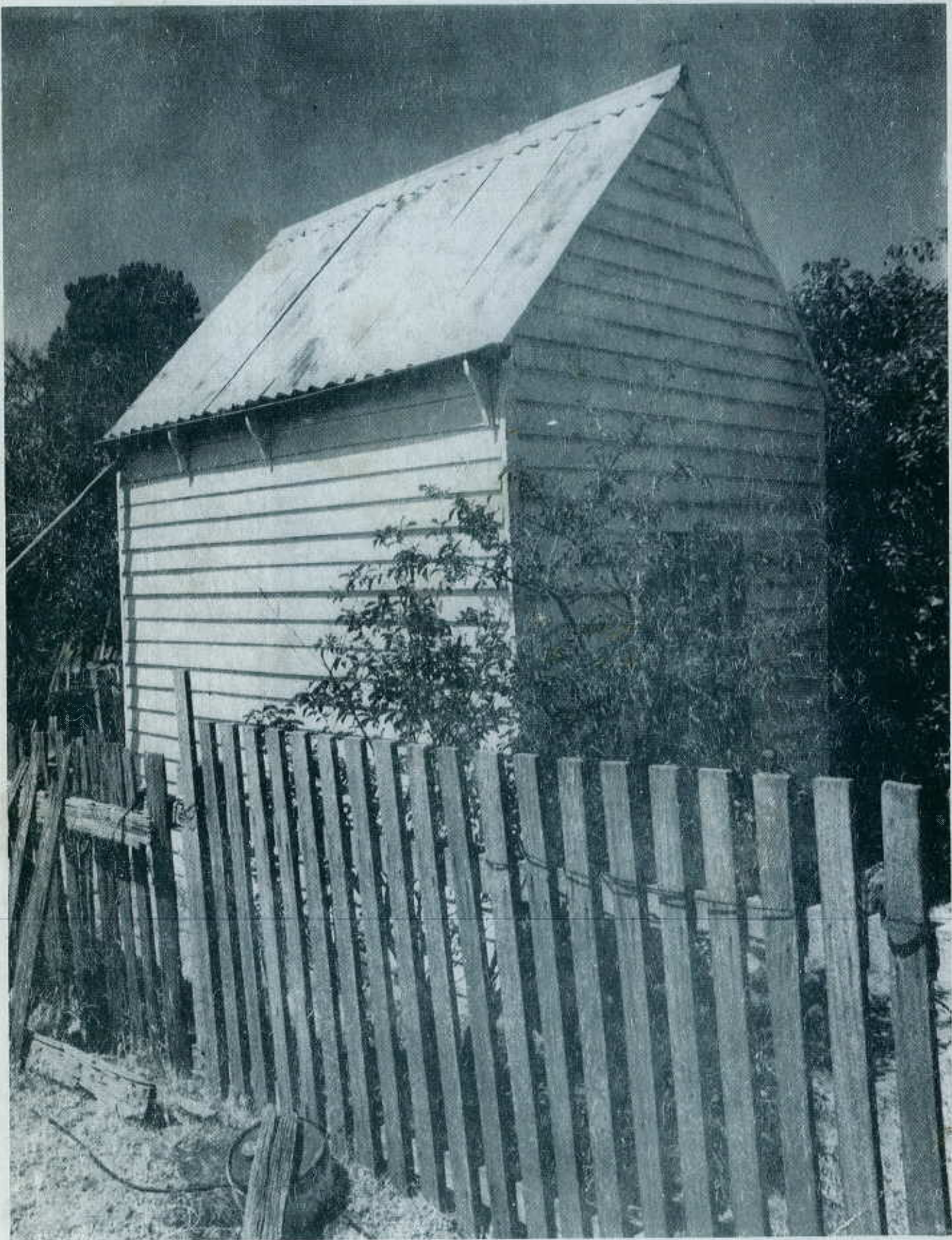


# **PORT ALBERT**



**GRAEME BUTLER**

# **PORT ALBERT CONSERVATION STUDY 1982**

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# SCHEDULE OF BUILDINGS AND SITES

No.	Name	Crown Allotment	First or Major Owner	Study Rating	Page
<b>ALBERTON TOWN</b>					
1.	House, Johnson Street	A/2	Charles H. Morse	D	15
2.	House, Johnson Street	3-6/3	William Saunders	B	15
<b>ALBERTON EAST (Orr Special Survey)</b>					
3.	Mareen, Bank Street	5/2	William Hair	C	16
4.	House, Bank Street	9/2	Matthew Goy	C	18
5.	Store and Residence Turnbull Street	19/2	William Hair	D	18
6.	House, Streleski Street (Brewery Road)	1 & 2/3	Frederick Shaw	C	19
7.	House, Kirksopp Street	17/3	John Whitford	C	19
8.	House, Danger Street	1/4	Marion Thomson	C	21
9.	House, Turnbull Street	1/7	William Hawden	D	21
10.	Victoria Hotel, Turnbull Street	3/9	E. T. Newton Dan Sexton,	C	22
11.	Eabon Eabon, Hawdon Street	2/17	E. T. Newton	B	24
12.	Former Alberton Butter Factory, Russell Street Contin.	4/17	Alberton Butter Factory Co. Ltd.	D	26
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14.	House and Site, Yarram-Alberton Road	63	James J. Matches	D	27
15.	House and Claypits, Yarram-Alberton Road	100	William Wigg	D	29
16.	House, Yarram-Alberton Road	103	Thomas Rooney	D	30
<b>ALBERTON EAST (Government Survey)</b>					
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18.	Wood Cot Park, Tannery Road (Myrtle Point)	28	James Neilson	A	32
19.	Farm House, Tannery Road, (Myrtle Point)	30	Anglican Church	B	36
20.	Sea Bank, Old Port Beach	45-8	Charles Tyers	A	39
21.	House, Tannery Road, (Myrtle Point)	105E	Patrick McGrath	B	40

No.	Name	Crown Allotment	First or Major Owner	Study Rating	Page
44.	Maritime Museum Former Bank of Victoria	A of 1/5	Bank of Victoria	A	69
45.	Rodondo and The Smiths, Formerly Flannery, Duplex, Tarraville Road	B of 2/5	Elizabeth Flanner	C	70
46.	Hall, Former Mechanics' Institute, Victoria Street	35/4	Port Albert Mechanics' Institute Committee	D	71
47.	House, Albert Street	2/4B	--	D	73
48.	Wee Waa, Queen Street	15/5	--	D	73
49.	St. John's Anglican Church Nelson Street	16A/10	Anglican Church	C	73
50.	Former Methodist Church, Tarraville Road	17/9	Wesleyan Church	C	74

#### TARRAVILLE GOVERNMENT TOWNSHIP

52.	Christ Anglican Church Tyers Street	3-6/1, 1a	Anglican Church	A	75
53.	House, Bridge Street (Robertson Beach Road)	2/4	Robert Webstel	B	78
54.	State School (SS 615) Stewart Street	9-10/6	Colonial Government	C	78

#### WORANGA PARISH (Reeve Special Survey)

55.	Ruinous House, Loughnan Street	-/23	Thomas H. Smith	A	79
56.	House, Former Commercial Hotel, Reeve Street	-/1	Matthew Absan	A	81
57.	Farmhouse, Loughnan Street	-	George D. Hedley	A	82
58.	Green Hills, Farm Complex, Ray's Road,	-	Edwin Ray	A	84
59.	Hawthorn Bank (formerly Lillies Leaf) Off Bland's Road	1	Gilbert Brown	A	86
60.	Farm Complex, Shaw's Road	7	Frank Gooch	D	89



held belatedly in August 1846, but a post office was gazetted over three years previously, and a Court of Petty Sessions established for the district in 1844. By 1856, the township's nine sections fronting Streleski (sic), Thompson, Sobeiski, Johnson, Gipps, Broughton and Phipps Streets were joined by six more sections on the south-east and a number of agricultural allotments were surveyed amongst the tea tree flats to the south-west: Union Bank, Junction and South Streets were the additional street names.

Below Edwards Grant's township extension of 1856, M. J. Campbell surveyed some 155 allotments varying from several four to eight acre blocks, facing Old Settlement Beach, to large rural lots such as J. Neilson's earlier 96 acres at the Tarraville Bridge, a small percentage of which were already alienated prior to the compilation of the parish. Amongst these lots were reserves for the existing burial ground and a proposed site nearer to Tarraville for the future; the Race Course of 153 acres was already established by then and a reserve was surveyed for it. The land quality was patchy with swampy areas abounding; the vegetation of the better parts being chiefly Gum, Cherry Tree, Wattle and She-Oak. This was Alberton East Parish.

Government inactivity in the supply of allotments for the rising population was more than countered by private speculation within the two Special Surveys. The boundaries of Orr's Survey were officially laid out by Assistant Surveyor Smythe in April 1843 and, in the following July some 80 half acre town lots were offered, from the Survey, at an upset price of 5 pounds per acre; twenty-three were sold. Of the suburban lots offered, two sold at 2 pounds 2 shillings per acre, whilst seven "cultivation farms" were dispatched at prices varying from 10 shillings per acre to twice that. This was the town of Victoria which adjoined Townsend's 1842 Alberton Town to the north; its north-south streets aligning in direction but not in name. Optimistically, the town's creators glorified their surnames.

Gipps Street (Alberton) ran into Hawdon Street (Victoria): Phipps Street became Orr Street; and Broughton Street communicated with Turnbull Street. Kirksopp, Rankin, Buckley, Gellion and Danger Streets comprised the rest of Victoria's avenues whilst its surveyor, Robert Russell, applied his name to a street which was to eventually adjoin the railway on the north.

Six town sections were adjoined by fourteen suburban sections, with lots of just over two acres each within. A store was built there early in 1843, and lots were soon fenced after purchase. By 1844 another store had been constructed and thirty, mostly timber buildings were dotted around the town.

However, by 1861, 4000 of the 5120 acres were as yet unsold and surveyor R. C. Bagot was commissioned to add some 142 country lots, of 20-40 acres each, north, east and west of Russell's plan. These were sold, together with unsold town and suburban lots from the previous subdivision, at an auction presided over by E. T. Newton, at the Alberton Hotel.

The other Special Survey was to remain undivided and largely as rural land for a longer period than Orr's Survey, but the leasehold township established there concurrent with Victoria, was to develop quickly because of its more passable land-link from Shipping Point to Sale.

Reeve advertised in the Port Phillip Herald (15-9-1843) that fifteen farms, each of 50 acres, were available for lease over a period of five years from his Tarasdale estate: the rent was set at 5 pounds per annum and it is probable that Robert Russell also surveyed their boundaries.

Despite the leasehold nature of the land tenure, two large stores were built and a brickmaker carried out his business there such that several substantial brick buildings were under way in June 1844. James Neilson is said to have built the large brick-built Royal Hotel at what had now become a village of some fifty buildings, mainly of brick, by late 1844.

Reeve Street was the non-central axis to this town of sixteen sections which faced streets named Albert, King, Wentworth, McCabe, Loughnan, Wood and Raymond. Reeve Street grew around the dray track which had been formed after Townsend's 1842 re-alignment of McMillan's first track. It proceeded via Tarraville and Woodside and eventually communicated with the Monaro District. A privately subscribed bridge was built at the Tarra River, in 1842, to complete access to the Port.

The Monaro track turned westward from Flooding Creek (Sale) in 1845 when Sergeant Walshe and a party of native police cut a track to Port Phillip; travelling north of the Latrobe River and avoiding the Great Swamp. Subsequent routes were refinements of this early Sale Road, but access overland to the cattle markets of Port Phillip, continued to go via Tarraville. It was cheaper by 1849, once the track was partially cleared, to drive the cattle overland than to ship them from Port Albert. Sale became the crossroads of these two options.

The swampy nature of the land between Alberton and Westernport determined that this overland route was avoided.



■ Tarraville Road, from Victoria Street (Latrobe Collection, State Library of Victoria)

■ Tarraville Road, from Victoria Street (Latrobe Collection, State Library of Victoria)



The private acquisition of Shipping Point was the turning point. Unexpectedly, surveyor Smythe, surveyed 180 acres within the five mile reserve, in June 1843, which included the invaluable Shipping Point; thus reversing the previous philosophy of the Surveyor General that the port should be kept in Government hands. A Melbourne Sale was advertised for September 1843 and the successful purchaser at the upset price of 180 pounds, was one Major Davidson.

Davidson, an Indian Officer, had arrived from Calcutta in the previous April and had been the guest of Turnbull, Orr and Co. until the sale. Not surprisingly he resold the land in a contract, dated November 1844, to the Company some months before receiving his Crown Grant. Prior to this, in March 1844, Turnbull, Orr and Co., had shown full confidence in the outcome when they relocated their store from the Crown Reserve, at the Old Port, to Shipping Point soon to be Port Albert. So too did they purchase one of Lord Glengarry's prefabricated buildings, from the Snugborough run, when Glengarry disbanded his model farm in c1844. This house was moved to Shipping Point where it commenced as the Port Albert Hotel in August of 1844.

Charles Tyers described the new settlement as being confined to ten acres of ground, the rest being swamp. Two wells had been sunk and yielded only salt water and the population of 36 people relied on carted water, no doubt from the Old Port. However, by December there was a sign of optimism as neat cottages began to emerge from the tea-tree. Tyers wrote that the Old Port was deserted by July 1844, despite the addition of Loughnan and Taylor's goods store there in the previous year.

A customs house was erected, reputedly by convict labour, at the new port, ironically on land that was leased from Turnbull, Orr and Co. W. A. Moore was the first officer to commence there in March 1844. The pilot Cpt. David Fermaner was appointed officially in January 1844, although it was claimed that he had been acting in this role informally since 1841. Further, Government services supplied, in return for the customs duties levied, were the appointments of a Landing Waiter and Tide Surveyor in 1846. In the same year Turnbull constructed a small jetty to the south of the small stockyard and slipways which had housed cattle for shipment, some years hence. These meagre facilities were the subject of many complaints from shippers of stock and produce; urging that a Government port be located at Welshpool or One Tree Hill to escape Turnbull's monopoly. One result was a town survey at Welshpool in 1851 which proved too remote from the established roads and did not develop quickly. A parallel but unconnected gesture was the provision of a permanently buoyed channel to the port by mid 1851 and the erection of a second-class double-fixed light on Snake Island (then Latrobe Island) in 1859.

With the post-gold boom of the mid 1850's, came the survey of H. B. Thomas dividing Turnbull's 180 acres into thirteen sections, each containing town lots of one to two roods in area. Turnbull had advertised in 1854 that these lots were available for 125 pounds to 1450 pounds each which appear to be inflated prices in comparison to those acquired at Tarraville, with improvements, in the same period.

Such were the prices of allotments that petitions sought Government acquisition of the port or the establishment of yet another Government town. It was the latter that was the subject of M. J. Campbell's survey of 1857, which created eleven sections amongst the heath, honey-suckle and gum trees. This was Palmerston.

Sites for an immigration barracks and a police barracks were included whilst, some time later, a court house site was reserved adjacent to the police barracks. Hence a further 110 lots were added to the already generous supply. By 1889, more suburban lots were added to the north and west of Campbell's town survey.

As with Tarra Vale, the effect of an additional supply of land was to encourage the sale of Turnbull's Port Albert sites. Alex Davidson had sold his 180 acres to Robert Turnbull and John Orr for a token 90 pounds in 1844, which had been realised two years later by payment from Turnbull, Orr and Co.'s mortgagers, Robert Hislop and William Jamieson who had in turn advanced 1993 pounds 18 shilling and 10 pence to the Company with the land as security.

By 1852 all of the land was owned by Robert Turnbull and mortgaged again, this time to the Savings Bank of Victoria; all except the allotment at the south-west corner of Albert and Victoria Streets which had already been sold to W. A. Moore, Collector of Customs for his dwelling. The actual sale of allotments increased markedly in 1855-6 when John Gellion purchased the Port Albert Hotel for 2000 pounds and David Fermaner purchased the site of the Ship Inn and his future residence in Wellington Street for 575 pounds.

Her Majesty the Queen paid 1200 pounds for the Customs house site in Wharf Street. Another Government purchase was at the South End of the point for a Government pier: Palmerston did not command access to the channel. In 1859, a timber wharf was constructed by Amos and Co. for 1616 pounds and thus began the increasing Government



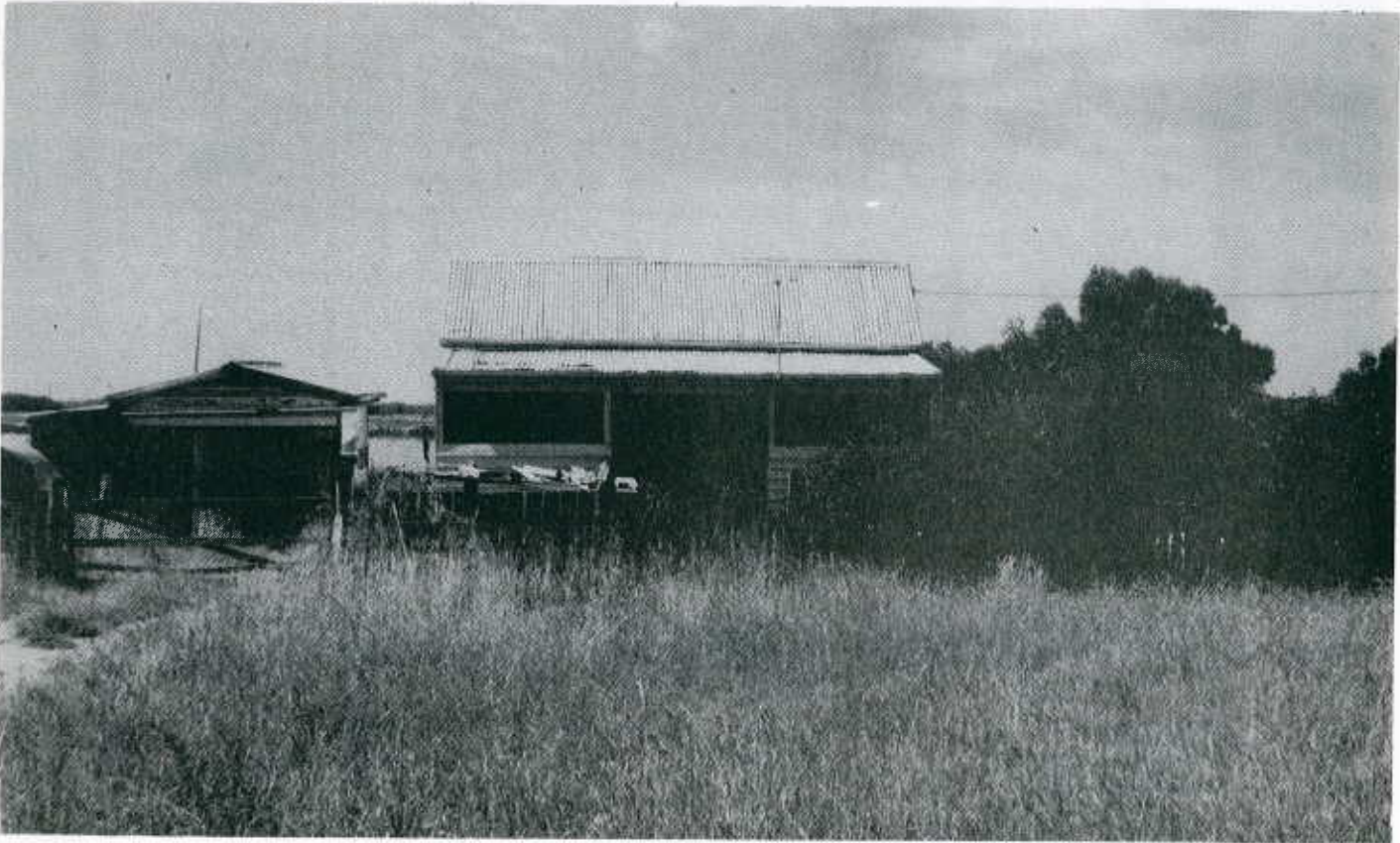
## ALBERTON TOWN

1. House,  
Johnson Street,  
Part CA 'A'12

*Agricultural Zone*

*First land owner: William Nairne, 3/12/1878.*

Thought to have been owned by Charles H. Morse, early this century. <sup>1</sup> He was a bootmaker with a shop in Johnson Street further to the west. It appears to have been built, contemporary to his occupation, around 1905. It is a contributive, original gabled and verandahed timber house with associated timber fencing on the adjacent land to the east which is of note.



2. House,  
Johnson Street,  
Part CA-3-6/3,

*Agricultural Zone*

*First land owner: William Saunders (blacksmith) 13/12/1883.*

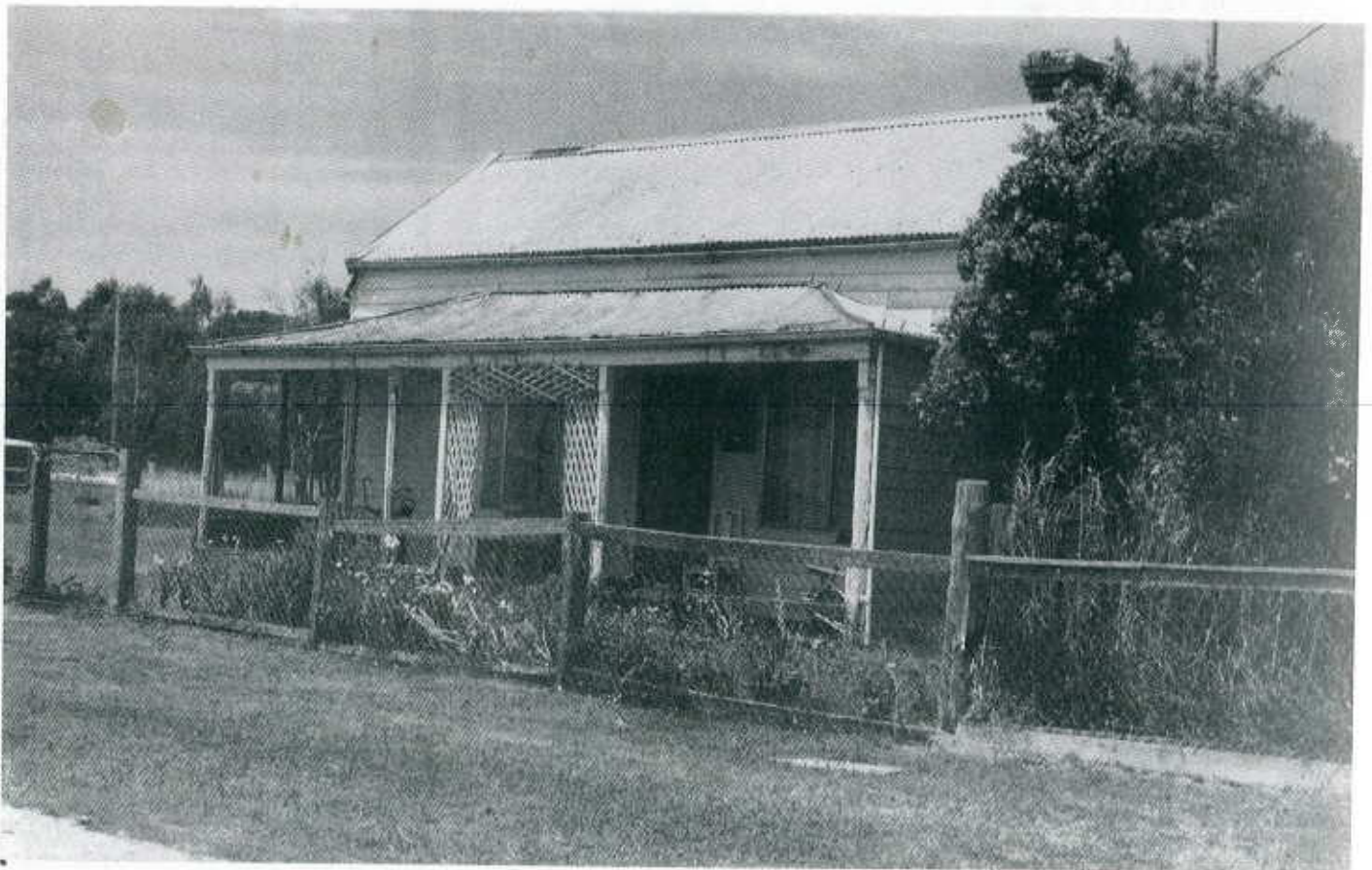
Soon after purchasing the land, Port Albert blacksmith, William Saunders, moved from the Flannery (46) after he had built this house in 1889. <sup>1</sup>

It is a large stuccoed brick house, with a slated hipped roof and a timber verandah with a concave roof. A timber section lies at the rear and probably served as a kitchen and scullery. Remnant planting is evident in a Golden Sage hedge at the road frontage.



■ 3: *Mareen, Bank Street*

■ 4: *House, Bank Street*



6. House,  
Streleski Street,  
Town Lots 1 & 2/3, Victoria  
Early land owner: Nathaniel Ford.

Agricultural Zone



During the late 1850's, John Orr sold these allotments to the Tarraville butcher, Nathaniel Ford. 1 The land remained vacant until after Ford's death when his widow, now Elizabeth Cauley, sold it to Port Albert storekeeper, Frederick C. Shaw: this was in 1885, for 16 pounds 15 shillings. 2 By 1891, Shaw had constructed this house 3 only to die, at Alberton, four years later. 4

It has a high M-hip roof with a concave roof verandah, stuccoed brick walls and cast-iron finished brackets to the timber capitals of the verandah posts.

The house is said to possess cavity wall construction both internally and externally on stone foundations. 5 It may have been designed by the architect, George Hastings, who was responsible for most of the substantial buildings in the area during the 1880's - 90's. (Refer 50).

7. House,  
Kirksopp Street,  
Town lot 17/3, Victoria  
Early land owners: James Cassidy, 10/5/1861.

Residential A Zone

James Cassidy purchased this land from John Orr, in 1861, and exchanged it with John Whitford for lot 44 of the same survey. 1 Whitford is rated for a house and land in Victoria in 1863. 2

A double-fronted, gabled timber cottage with a bullnose verandah roof and distinctive scrolled timber brackets. Fencing has been replaced as has the side wall cladding with asbestos planks. It is contributive to the character of Victoria and may be one of the few buildings left from the 1860's, despite renovations.

8. *House,  
Danger Street,  
Suburban lot 1/4, Victoria.  
Early land owner: Nathaniel Ford.*

*Agricultural Zone*

Robert and Marion Thomson appear to have built this timber house c1903.<sup>1</sup>

The house has twin, gabled bays with a cast-iron verandah (columns, frieze and brackets) spanning between. Intact picket fencing and planting complete what is an original example from the era.



9. *House  
Turnbull Street,  
Town Lot, part CA1/7, Victoria  
Early land owner: William Hawden.*

*Residential A Zone*

This house is thought to be the site of George Collis's hutchers shop of early this century and is believed to have adjoined a cordial factory on the north (Gleeson's).<sup>1</sup> This has not been ratified.

The house has a double gabled roof of sheet iron which was formerly striped and is of a pitch which suggests shingles originally clad the roof. A face-brick, corniced chimney is at the intersection of the two gabled bays, indicating staged construction, whilst the double supports to the verandah may have been originally panelled or trellised: the verandah roof is concave in profile.

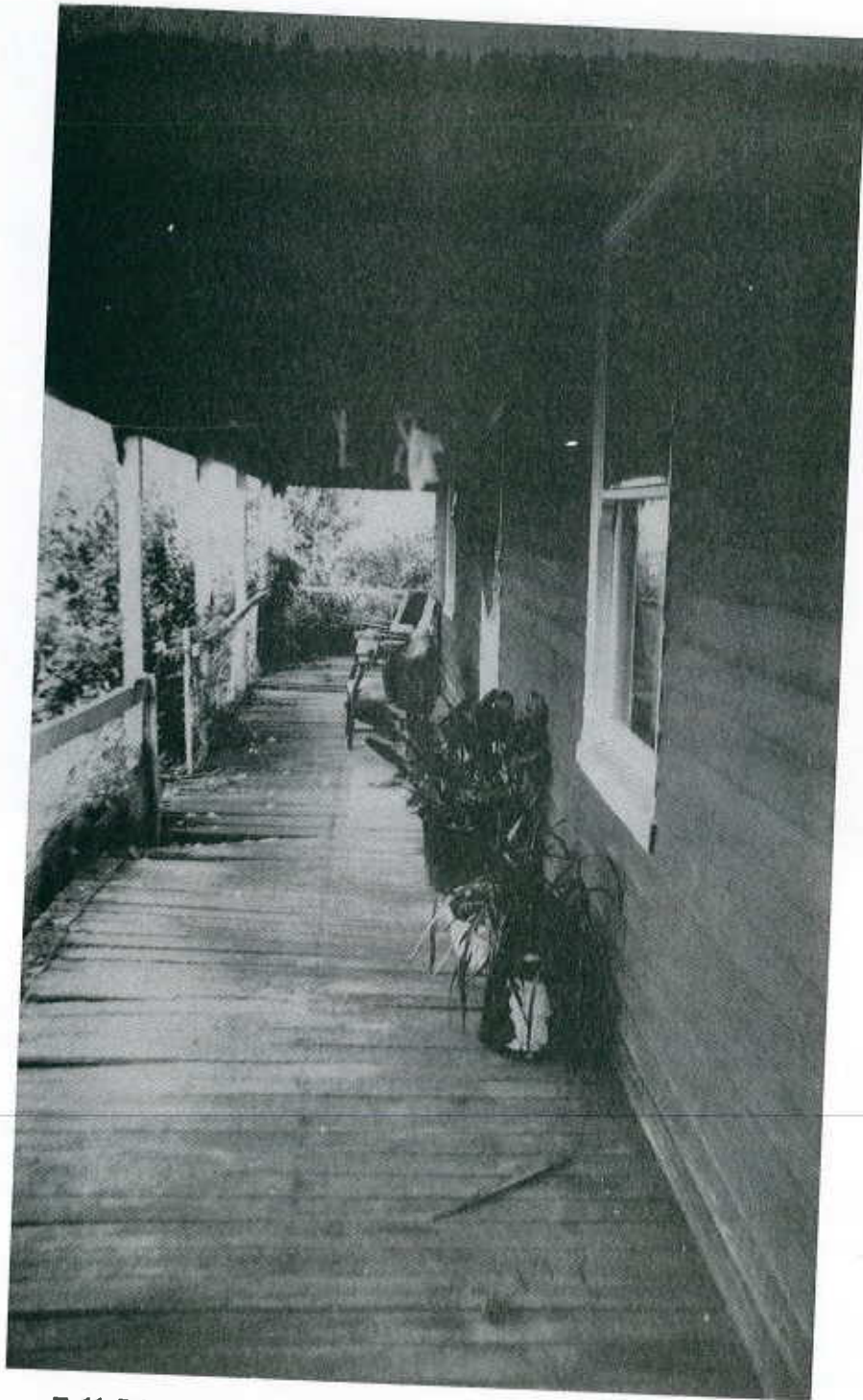
The assymetrically placed gabled bay may have been added and the fences have been removed, but otherwise the house is relatively original and indicative of buildings of the 1860's. Thus it is an important element of the surviving character of the old township.



*(Port Albert Maritime Museum)*

*(Cox's Notes, Latrobe Collection, State Library of Victoria)*





■ 11: *Eabon Eabon*

13. *Farm House,  
Yarram-Alberton Road,  
Part CA 63,  
Early land owner: Charles Forrest.*

*Agricultural Zone*

John Orr sold this allotment to Forrest in 1846 for 51 pounds 13 shillings and sixpence, John Martin owned it soon afterwards. 1

Jacob and Edward Martin, acting as John's trustees after his death, sold part of the allotment to John Casbolt (15 acres) in 1883. 1 This was adjacent to land already owned by Casbolt, to the west of which no record has been found.

Casbolt operated a brickyard on this land from at least the mid 1880's 2 and the clay holes may still be seen to the west of the house yard. Some time early this century, the Casbolts built this house, no doubt using bricks of their own making.

The house has a M-hip profile roof, is double-fronted with timber cladding and has a return verandah of timber. The verandah roof is convex in profile: this and the scrolled brackets date the house as early this century. The fencing and roof cladding have been altered; the latter obtrusively. The house is contributive architecturally and, with the associated clay pits, illustrates the brickmaking aspect of the area's building industry.

14. *House and Site,  
Yarram-Alberton Road,  
Part CA 63,  
Early land owner: Charles Forrest.*

*Agricultural Zone*

Thomas Martin, with his family (John, Jacob and Edward), arrived in the Colony from Launceston during 1839, taking up the Little Red Bluff (Point Ormond) Pastoral Lease at Western Port 1840-1. Another Martin held a lease at Brighton, from 1840 and was dispossessed by Dendy's Special Survey there in 1841.1

Reputedly the Martins drove their cattle from Brighton in 1843 to 200 acres, purchased in Gippsland. 2 However, The Pines, at Orr's Special Survey was first purchased by one Charles Forrest in 1846, from John Orr, together with buildings and fences relating to pastoral occupation. It is unknown when the Martins purchased The Pines, but in 1859, Jacob and John were to be compensated for the clearing of the Yarram Yarram road through their land 4 and in 1861, it was marked as sold on Bagot's subdivision plan, to J. & W. Martin. 5 Thomas died on Orr's Survey in 1856, 6 and John, his son, in 1883. 7 Edward and Jacob, as John's trustees, sold allotments from The Pines to John Casbolt in 1883 and James McKay in 1857. 8

Both Jacob and John were rated in 1863 for a house and land each on the Victoria Survey: 9 in 1868 John was credited as a dairyman whilst Jacob was a farmer. 10

Late last century, Ernest Buckley acted as a trustee for the land, 11 leasing it to Frederick Warren in 1900 12 and selling it to J.J. Matches by 1901. 13 Duncan Matches owned it from 1906 until its purchase by the Finlay family c1946. 14

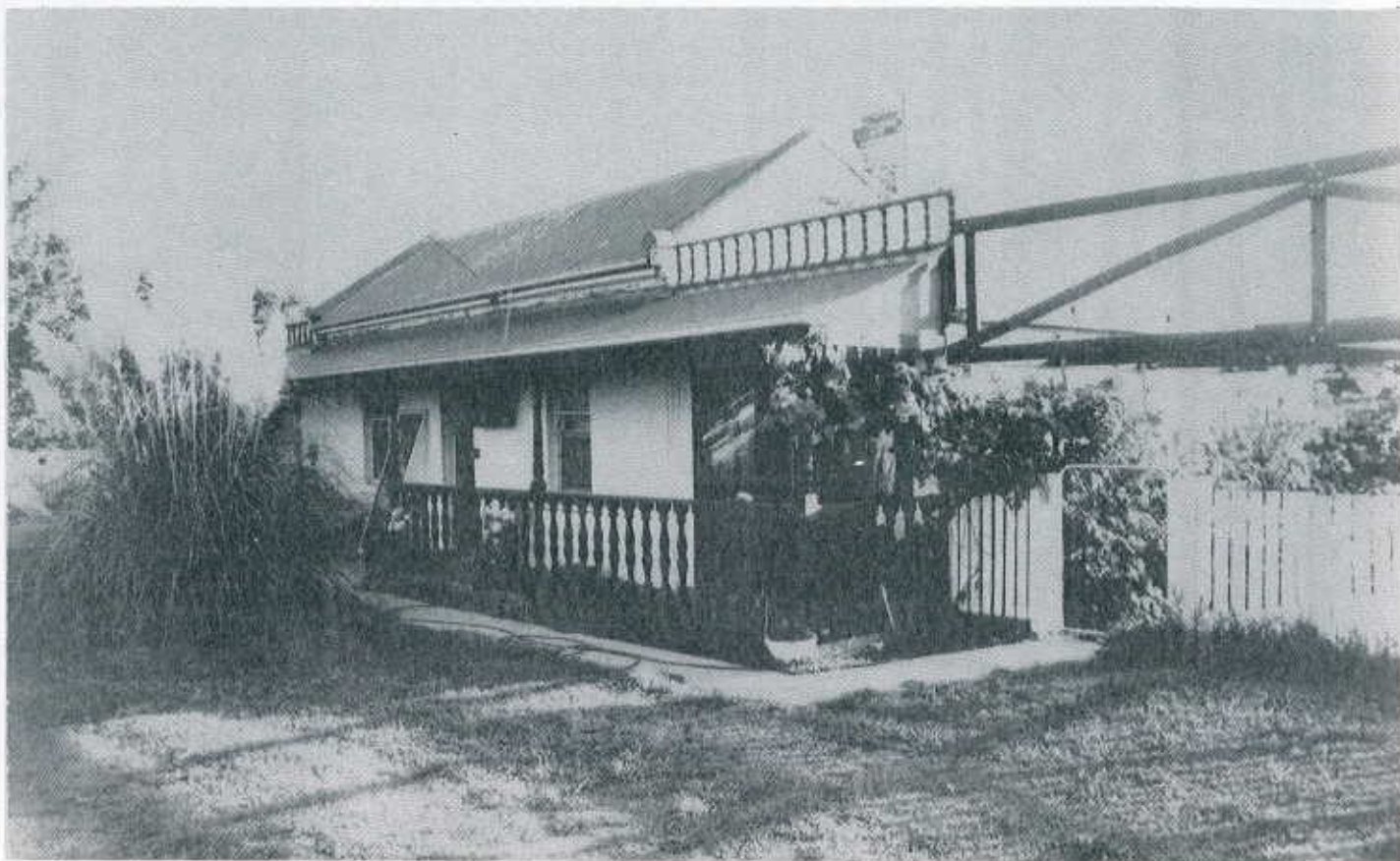
Two ruinous timber houses remain on the site today; both have sawn timber framework and sawnboard cladding. They appear to date from around 1880-90 although evidence exists of hessian and paper on one, whilst hand wrought nails are used throughout; the floor of the former is gone and renovations have been carried out sometime this century, with the sheet plastering of walls and provision of new architraves.

Extensive rows of Monterey pines have been planted on the property last century whilst two Norfolk Island pines near the house have been planted, more recently, around 1900. The Martin's "dog roses" which once lined the Yarram road on both sides, as described by the Rev. Cox early this century, have been recently eradicated by the Lands Department of Victoria, as "briar roses".

The site was the second freehold to be sold from Orr's survey and the first rural allotment purchased. Hence it is one of the early freehold farms established in Victoria. The pines which symbolise its name have nearly reached the end of their life.

15. *House and Site,  
Yarram-Alberton Road,  
Part CA 100,  
Early land owners: Margaret Dunn, Daniel Sexton.*

*Agricultural Zone*



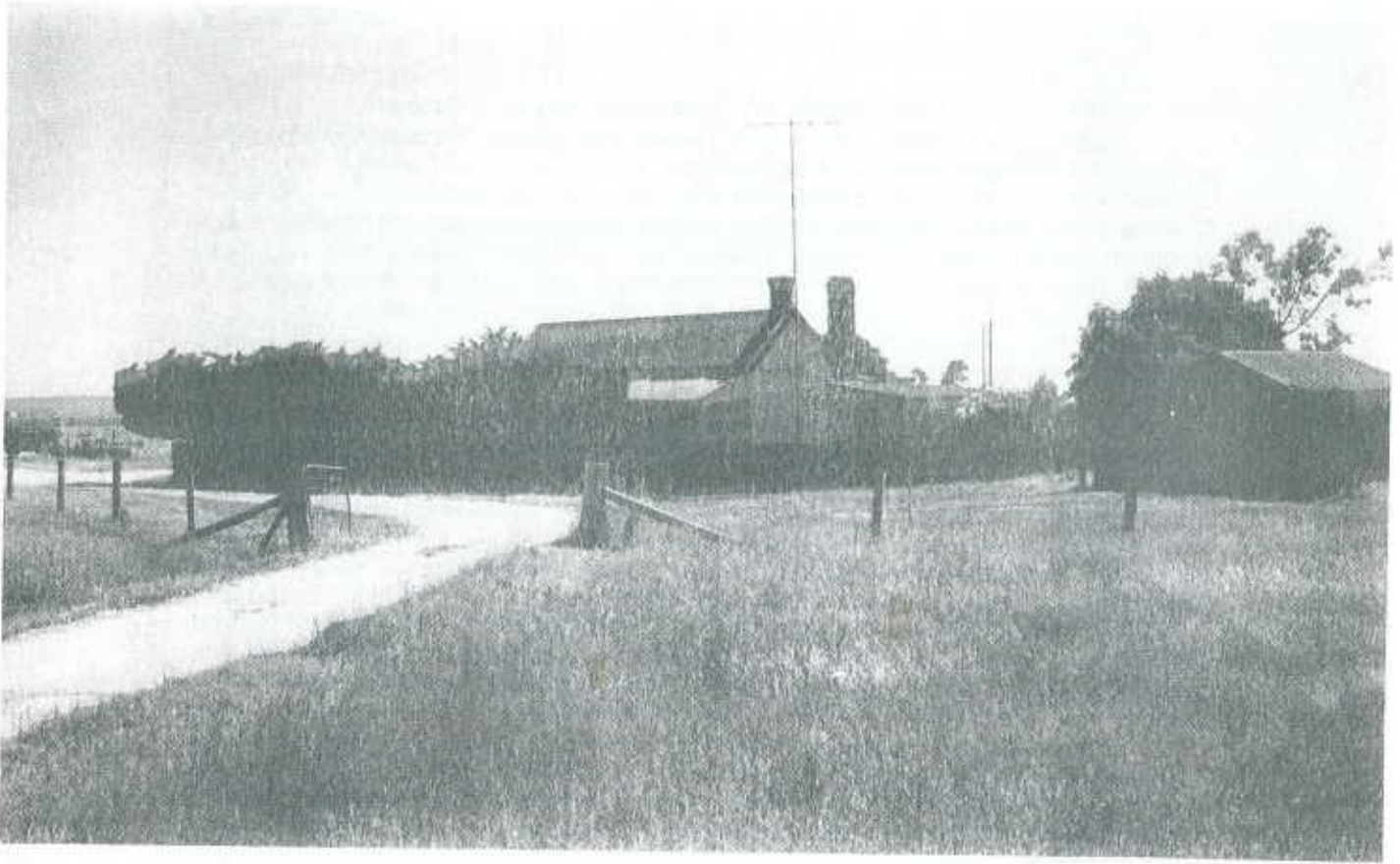
Brickworks were established during Alberton Tarraville and Port Alberts' period of expansion during the 1850's whilst brick building had been carried out, prior to this, during the 1840's in Tarraville. The 1846 census disclosed six buildings of stone or brick of a total of 122 in the area.<sup>1</sup>

Builder, Samuel Taylor, started the Port Albert Brickworks in 1859 and an Alberton Brickworks also operated in this period under Bellingham and Co.<sup>2</sup> Brickmaker, John Casbolt Snr. purchased 15 acres from Edward and Jacob Martin's, The Pines, in 1883 to extend his holding purchased previously from James McKay.<sup>3</sup> The existence of clay holes at this site may verify its use as a brickworks.<sup>4</sup>

Meanwhile during another expansionary period, William Wigg commenced a brickworks on three acres of the land originally purchased from John Orr by Dan Sexton and resold, to Margaret Dunn in 1871: this was some distance to the north of Casbolt's and began in 1900.<sup>5</sup>

The cottage is double-fronted, of face-brick (painted) with a gabled parapet and brick chimney cornices. The concave-roof verandah has been rebuilt with non-period ornamentation added. These alterations and the painted brickwork detract from the house's significance. The claypits survive to the north of the house, exemplifying the building industry of the area (see 13).





■ 17: House Tannery Road

■ 18: Wood Cot Park



brought by the discovery of gold, after ten years of development in the district, generally and a lesser period of establishment by its owner, James Neilson who himself is linked with the first period of Gippsland Settlement and one of the more romantic tales of early settlement in Victoria, in respect to the Lord of Glengarry.

The house is perhaps the only existing example of an "oil-cloth" clad building and attached, at the rear, is a prefabricated iron-frame building which is one of relatively few to survive in-situ, in Victoria.





■ Wood Cot Park interior, iron framehouse

■ West elevation, iron framehouse (sheet iron to north wall)



elevation has been partly enclosed. A photograph from c1910 shows the tonal values of the original colour scheme (dark capitals, frieze and window joinery; medium walls and light posts and bressumer) as well as the original garden viz. gravel pathway to a central circular parterre fronting the house and several *Cordyline Australus*.

The house is a well preserved and substantial example of an unusual design, whether considered in an urban or rural context, and may be the former Tarrville Anglican parsonage: being definitely on the intended site of the parsonage. It is one of three houses each of an early design, grouped at this road crossing.



■ 19: Farm House, Tannery Road

20. *Sea Bank*  
*Old Port Beach,*  
*CA 45-8,*

*First land owners: J. A. Stratton (47-8)*  
*J. McKenzie (46),*  
*A. McMillan (45)*

*Agricultural Zone*

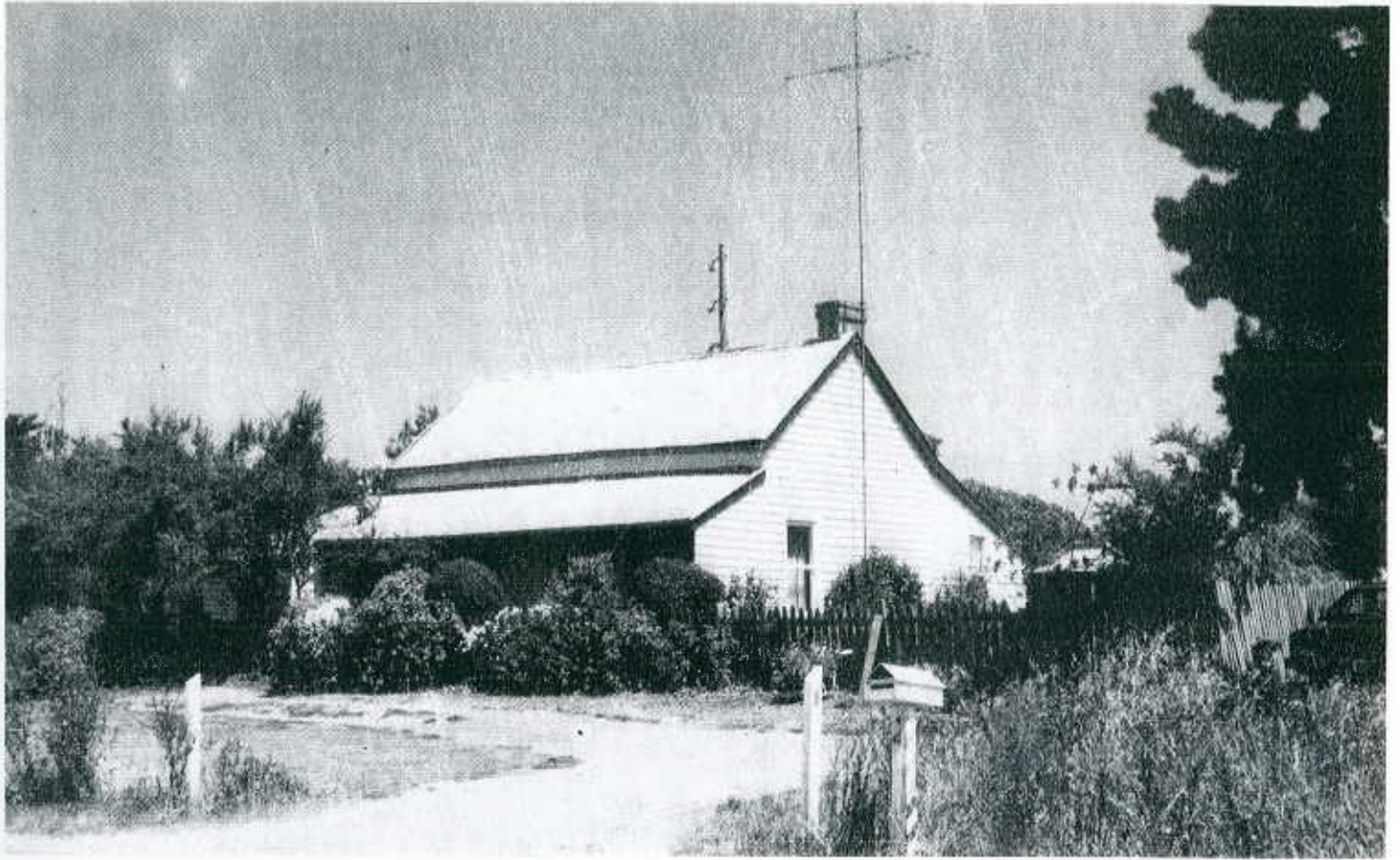


After Charles James Tyers was appointed Police Magistrate of the Alberton district in 1858, he sold his former property Myrtle Downs (½ mile from Sale) <sup>1</sup> and called tenders for the construction of a weatherboard cottage at "Commissioner's Point", Old Port, which was to be to the design of Port Albert architect Henry Thomas. <sup>2</sup> It was presumably nearly complete by September when Tyers called for tenders to build a two-rail post and rail fence and a broad paling fence around his property. <sup>3</sup> The cost was estimated at 400 pounds and to meet this, Tyers mortgaged the property in January 1859, for 750 pounds sterling. <sup>4</sup>

Meanwhile, Tyers wrote of his approaching occupation of Sea Bank on the first of October 1858, when the Rev. Bean was to spend a month staying with him: a bathing place was being made ready, built to keep out sharks. Tyers considered that "in the course of time (Sea Bank would) be a most valuable property." <sup>5</sup>

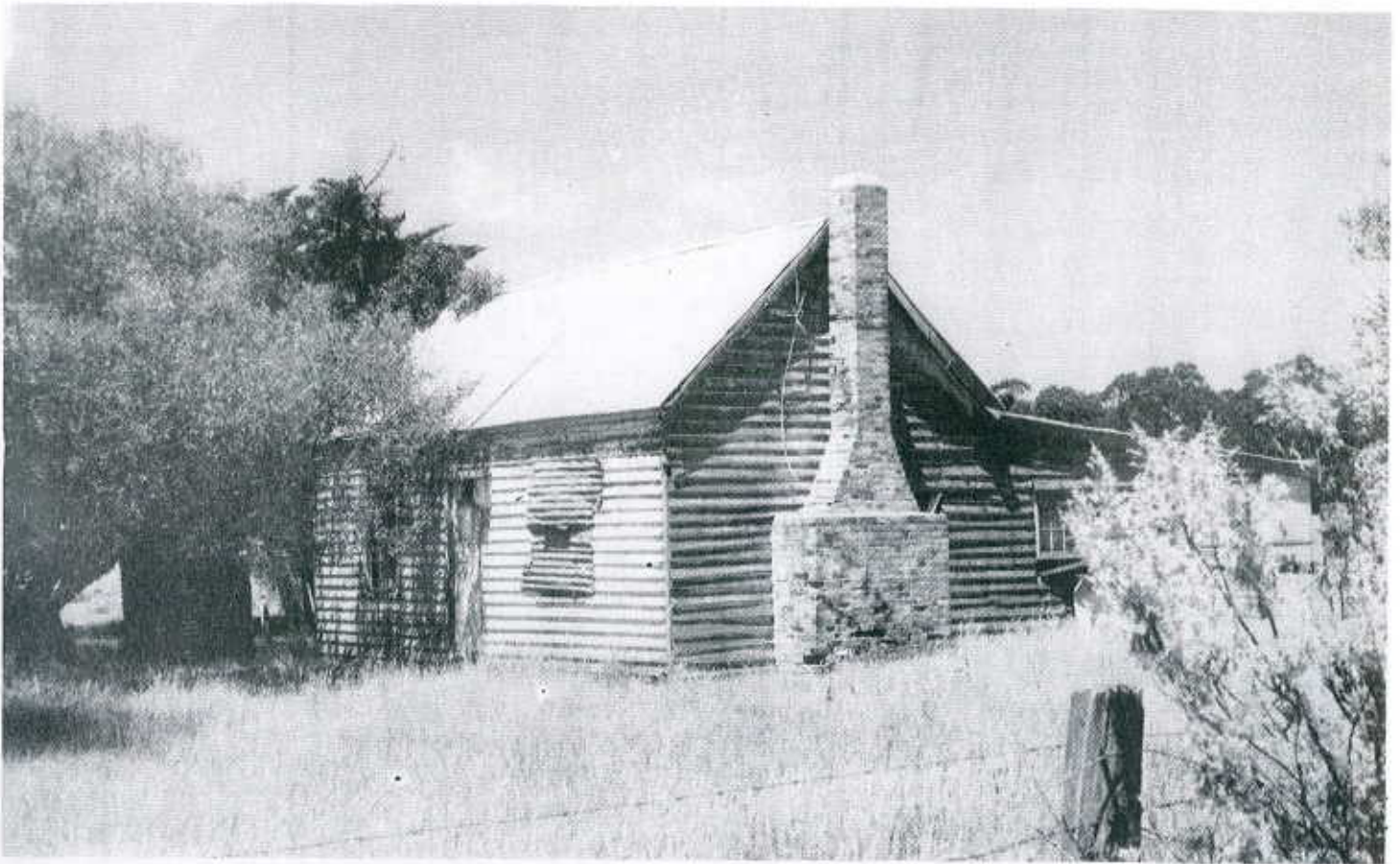
Financial difficulties may have forced Tyers to forsake his Sea Bank as early in 1859 (apart from the mortgage) he advertised Sea Bank for lease over a period from six to twelve months. It was described as having three sitting rooms and four bedrooms, a detached kitchen and servant's apartment. A store and spare room were nearby. <sup>6</sup> Although Tyers remained as Police Magistrate and in addition assumed the role of Warden of the Goldfields in 1861, <sup>7</sup> Sea Bank was still being offered (with 22 acres) for lease at the end of 1861, as were a number of large houses in the area. <sup>8</sup> A further mortgage occurred in late 1859 by when a wharf had been constructed as well as the swimming area. <sup>9</sup> This wharf existed until relatively recently when it was partially demolished by vandals. <sup>10</sup>

Charles Tyers, surveyor and public servant, was born in London, 1806. He entered the Royal Navy in 1828 and became known as an expert on marine surveying. At the rank of Captain he left the Navy in 1839 to become a surveyor under the Colonial Government; being requested to fix the boundary between the Port Phillip District and South Australia. He was Chief Surveyor in the Portland district from 1841 where he laid out the town and surveyed the bay; becoming a Land Commissioner in 1842. By 1843 he was appointed as the first Land Commissioner in Gippsland arriving there in January 1844. He assumed virtual



■ 21: House, Tannery Road at Myrtle Point

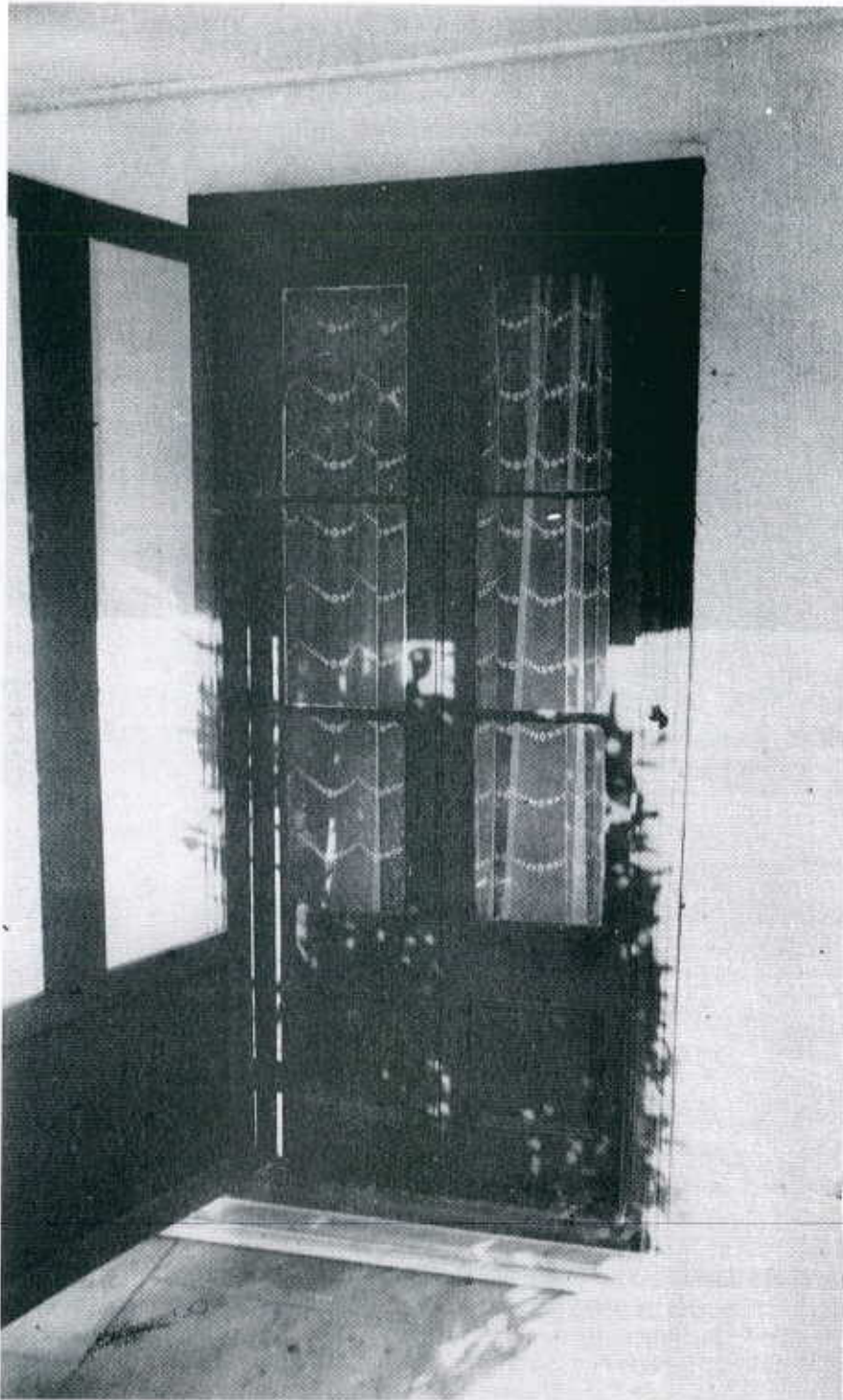




■ 23: House, Old Port Road

■ 24: Tarawonga, Tarraville - Port Albert Road



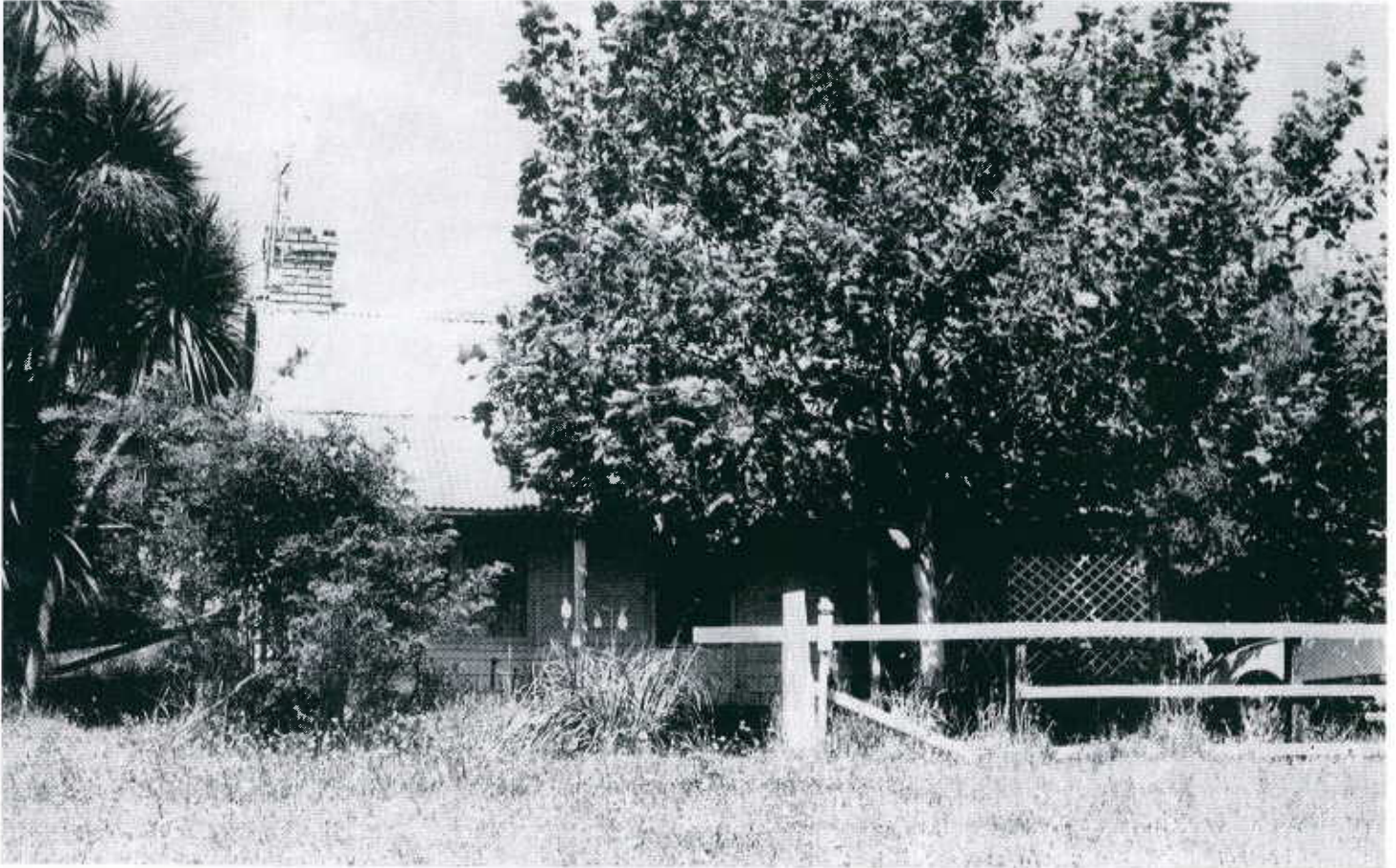


■ 24: Tarawonga, Tarraville - Port Albert Road



26. House,  
Brisbane Street,  
CA 1/2, 0.1.38  
First land owner: Ellen Gebbett, 11/10/1859.

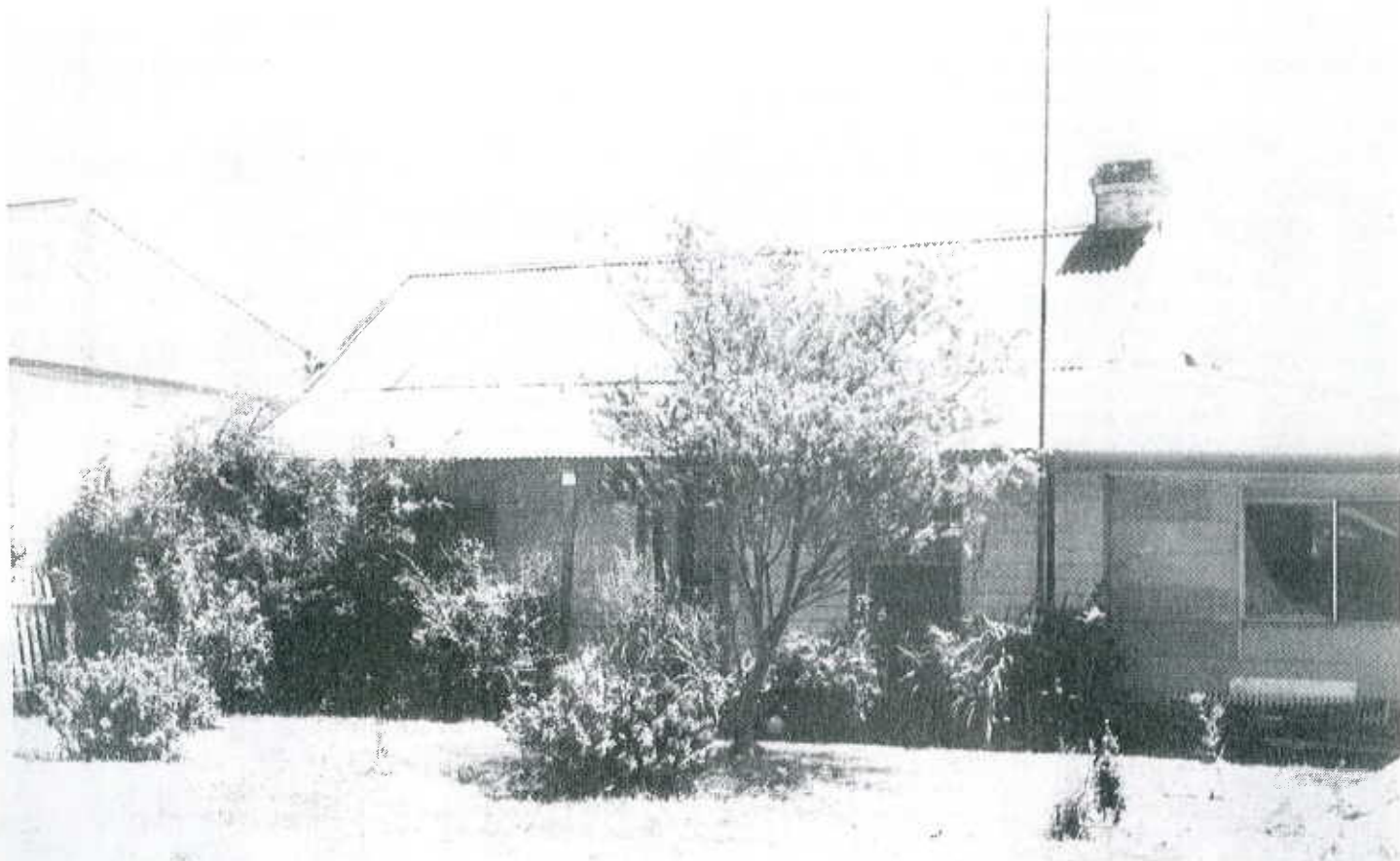
Residential A Zone



The carrier, William Gebbett and his wife Ellen, owned a house on this allotment in 1863. <sup>1</sup> William was listed in Butler's 1866 Directory in Gibson Street (sic), <sup>2</sup> but not in the Post Office Directory of 1868. <sup>3</sup> By early this century, the house and lots 1, 4-8/2 belonged to George Frederick Belcher Langhorne; gentleman. <sup>4</sup>

A photograph of the house, from the Langhorne Collection, shows its condition early this century, to be basically similar to the present. The house has a double gable roof form, is clad with weatherboards and the verandah roof a continuation of the main roof slope. Timber verandah posts support a simple frieze which curves to each capital. An unusual aspect of the house is the double, six-pane casement windows at either side of the doorway; the door itself having been replaced. A post is missing from the north verandah end and part of the frieze is gone. A tall (c1400) arrow-head picket fence and wicket gate once existed at the front, whilst of the two Cordylines, one remains.

The house is not as well maintained as other examples in Palmerston (25), (29), but it dates from the town's commencement.



■ 28: House, Denison Street

■ 29: House, Fitzroy Street



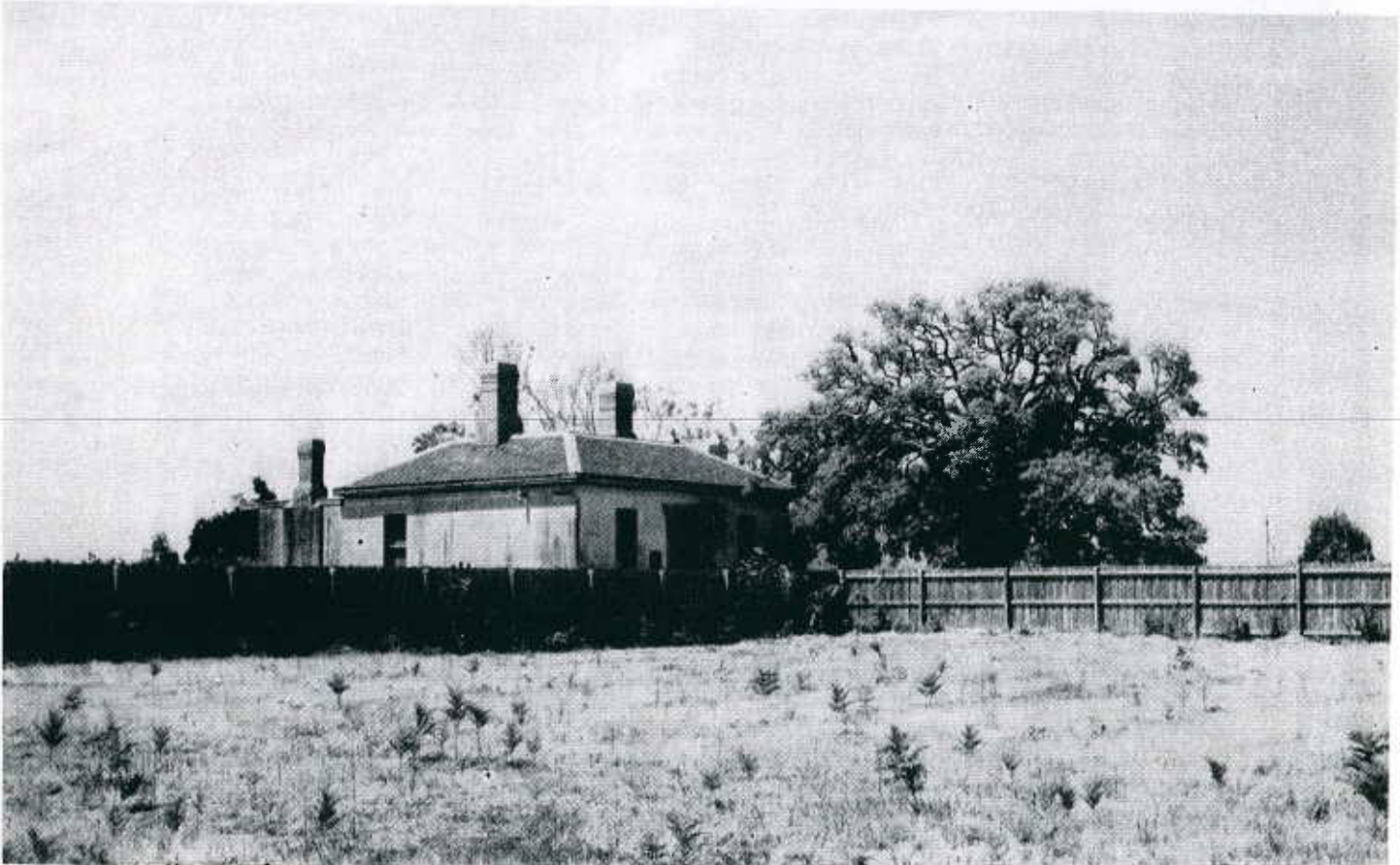
31. *Former Police Station,  
South Street,  
CA 4/4, 0 acres 2.0*

*Residential A Zone*

*(CA-3-4/4 originally reserved for Immigration Barracks in 1857 Survey)  
First private land owner: (3/4) M. H. Brosche, 26/11/1940.*

Initially allotment two, section five was reserved as a police barracks, a gaol and office. 1 Subsequently, a portion of the reserve created for the Immigration Barracks was used to construct police barracks which were replaced by the present police quarters of stuccoed brick, during 1886-7. 2 The drawings were prepared in October 1886 under Public Works Chief Architect, Henry Bastow, and builder William McKerrow completed the work at a contract price of 518 pounds 13 shillings and six pence. 3

The original building had a skillion-roof timber verandah, on the south elevation, which was supported on duplex posts at the ends and single supports between. Cast-iron brackets provided the decoration, resting on the timber column capitals. This verandah has been removed. Otherwise the house is near to original externally with the, as yet predominantly unpainted stucco, the M-profile slated hip roof and chimney cornices are generally complete (currently being renovated). This building has historical significance as one of two government-built structures from the government township of Palmerston.



## PORT ALBERT TOWN (Robert Turnbull Subdivision)

33. *Footings of former Powder Magazine  
off Bay Street, near East Street.*

*Foreshore*



An act passed in 1857, required all persons possessing more than 100 lbs. of gun powder to store it in a government magazine for safety. 1 As a result magazines were constructed at the towns of Fryerstown, Beechworth (1859-60), Heathcote (1864) and Eaglehawk (1866), being proximitous to gold fields and at Port Fairy (1860) allegedly for defence purposes. 2

Port Albert also was the port serving the Omeo, Baw Baw, Jordan and Dargo goldfields and hence a contract was let to Thomas Alpin and Co. to construct a powder magazine, in 1866, for 786 pounds 14 shillings. 3 Later in the year, J. Robertson and Co. won the tender for a jetty, to serve the magazine, for 295 pounds. 4 Like Castlemaine (1867) and Heathcote (1864) it was a simple gabled roof, bluestone structure with buttressed walls, whilst the jetty was 192' 0" in length, to gain the deep water at a hammer-head landing. 5

Today only the basalt footings remain and a few piles of the jetty. However it is a last tangible reminder of the gold activity in Gippsland which, apart from Walhalla, is not evident in a material way (substantial deep lead mining, entrance structures, ore crushers, etc.).

34. *Government Wharf,  
Wharf Street, Waterfront.*

*Public Purposes Zone*

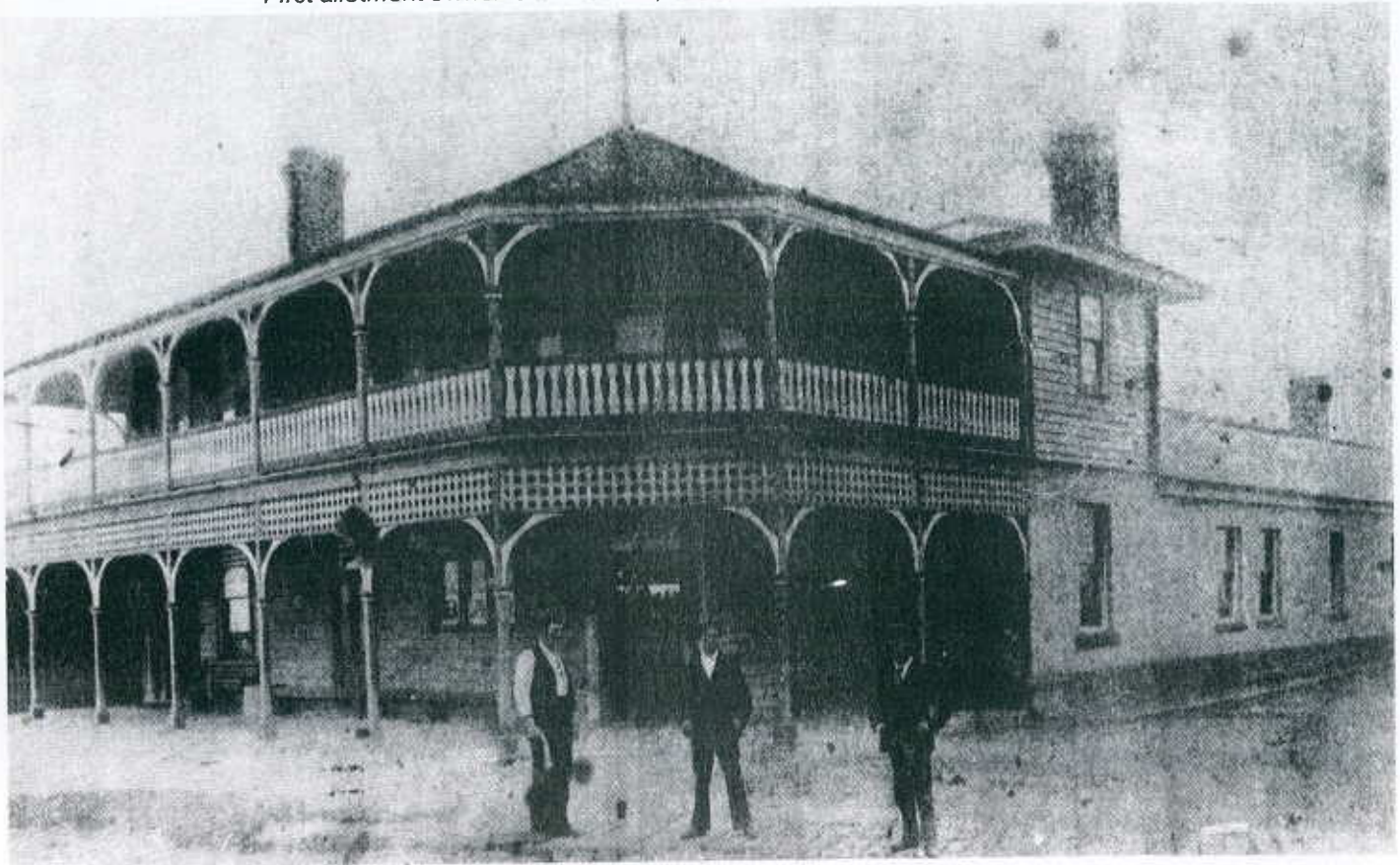
By 1842, the increasing numbers of convicts sent to Van Diemens Land required the supply of meat from nearby ports. Cattle were previously driven overland from the high plains in the late 1830's early 1840's, and English investment in Gippsland had almost ceased when wool prices faltered in England during 1837. Hence Port Albert was fortunate to be in an ideal location to supply the VDL market; being three days' sail from Hobart, whilst for sheep farmers the rising tallow trade in London began to replace the failing wool market as a further export. 1

Robert Turnbull arranged for the construction of a small jetty and stock-yards at Port Albert for Bells and Buchanan in 1846, and charged for its use. 2 An unofficial pilot service was commenced in 1841; being sanctioned by the government in 1844 and augmented in 1847. 3 Port Albert was declared a point of entry and clearance in October 1850 where

35. *Port Albert Hotel,  
Wharf Street,  
CA 1/1*

*Industrial A Zone*

*First allotment owner: John Gellion, March 1856.*



■ *Port Albert Hotel c1902-8 (Port Albert Maritime Museum)*

Lord Glengarry (Aeneas Macdonnell) proceeded to Gippsland in 1841 to set up his model farm. 1 One of the prefabricated buildings he brought with him was reputedly taken to the old port, after the farm's failure, having been purchased by the merchants, Hunter and Watson. 2 Here James Aitken, in 1841, operated a store and what may have been an unlicensed inn after its sale to Turnbull Orr and Co. It was transported to the present Port Albert in 1844 and William Howden became the first licensee by February 1844 of the Port Albert Hotel. 3

Behind the prefabricated building, a face-brick section was built in 1859-60 to the design of Port Albert architect, H. B. Thomas; being the only survivor of a fire in April 1893. 4 This section has been stuccoed since. 5 Grazier, Richard Bennison took the licence in April 1848 and another grazier, John Gellion followed him in 1851. 6 Gellion eventually purchased the hotel from Robert Turnbull in March 1856, for 2,000 pounds. 7 Under Gellion's ownership George Graves, David Watson, Thomas Donohue, Henry Taylor and Albert James Smith, leased what was described as a 13 room timber hotel with two or three outbuildings. 8

After John Gellion's death in 1884, the owner becomes The Gellion Brothers and Albert Smith continued as licensee until 1900 when he acquired the property to sell again to M. J. and R. H. Ward in c1910. During the 27 year period of Smith's tenure, only one other licensee, George Ellis, held the license briefly around 1890. 9

After the 1893 fire, the front section of the hotel was rebuilt in its present form c1903-4 by Albert Smith: Smith himself serving as Alberton Shire President 1898-9. Mrs. M. M. Smith was the licensee in the period 1902-8 and Albert returned in 1909-11, completing 26 years in the hotel. 10

The main section is weatherboarded of two-levels, with a two-level return verandah which terminates on an asymmetrically planned bay to the west and is open-ended on the east. The roofs are typically a series of sheet iron clad hips with a gable extended over the cantle corner of the verandah. Red face-brick chimneys of an Edwardian pattern, rise over these roofs, whilst generally the cladding details also derive from this period. The verandah balustrading is of flat timber balusters fretted to the shape of the turned variety whilst the trellis frieze below the first level and the open timber bracketing have since been removed. 11

An escape stair has been added within the verandah, the general colour scheme changed and the rear brick section stuccoed. Internally, only the upper level and the stair hall are original.

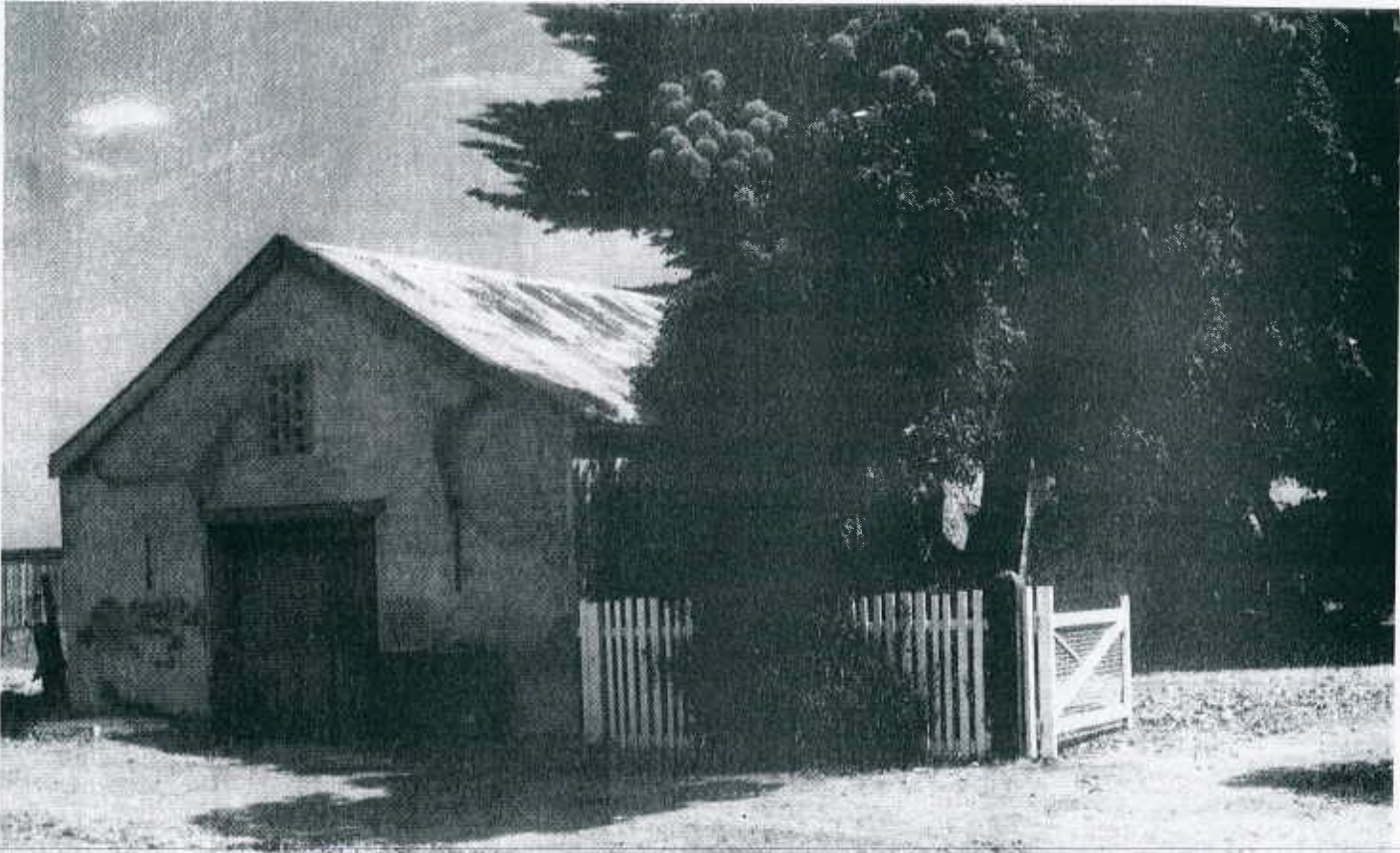
This hotel occupies an historic site in respect to the localisation of the first building activity of the new port at the stockyards and the establishment there of Gippsland's first licensed inn. Since the passing of stock shipments from the port, the hotel serves another industry, that of fishing. The hotel is one of the older, continuously licensed premises in the State.

Architecturally, the later building, although incomplete, propagates the building type built at many seaside locations early this century, (e.g. The Terminus Hotel, Point Lonsdale) and owing to its timber construction, provides a picturesque element at the focus of the township.

36. *Former Offices and Stores,  
Wharf Street,  
CA 3, 5/1*

*Industrial A Zone*

*First allotment owner: William Bellamy Howden, September 1862.*



Major Davidson still owned the 180 acres comprising Shipping Point when Turnbull Orr and Co. commenced the erection of a store building there in March 1844. By June, the store was complete. <sup>1</sup>

One of Wilkinson's survey plans of 1848 shows the site of "Buchanan and Co.'s Store" adjoining a timber yard, later to be the Customs House site. <sup>2</sup> The existing brick store is not shown whilst what may be the first stage of the existing brick offices is. H.B. Thomas' survey of 1854 shows a similar complex with an extension at the rear of the offices. <sup>3</sup> In December 1855, H.B. Thomas (who appears to have been Turnbulls' only architect at Port Albert) called tenders for the labour to construct a "Brick Store" for R. Turnbull Esq. <sup>4</sup> By October of the following year, Turnbull and Orr announced that their store was "appointed for free warehousing". <sup>5</sup> In a letter from Turnbull to Bells and Buchanan in December 1858, Turnbull differentiates between his stores by their construction stating his intention to sell spirits in their wooden store in Wharf Street. <sup>6</sup> This was probably the store erected in 1844. Given Turnbull's repeated anxiety about the future of the port and the expenditure needed to make small alterations during the 1840's, it is most unlikely that any substantial building

would have been erected prior to the gold brought prosperity of the 1850's. One "Bow Bells" in *A New Chum's Opinion of Gippsland* wrote that in 1861, during his recent visit to Port Albert, he had seen ... "a much better warehouse than any other in existence there, is being built ..." the rest of the stores were "tumbled down".<sup>8</sup> This may refer to further tenders called by H. B. Thomas for a Brick Store at Port Albert at the beginning of 1859.<sup>9</sup> By 1861, William Howden had been admitted as a partner to Turnbull and Co.

A panorama of Wharf Street dated by the Rev. Cox as 1863<sup>10</sup> shows the existing brick store and the first stage of the offices. It is likely that, given Bow Bells' description, the first tenders (1855) related to the offices and the second for the store (1859). An 1873 drawing shows the same panorama, but with the offices extended and parapetted as existing.<sup>11</sup> After a 27% drop in NAV, from 1869-70, a 5% increase from 1870-1 indicates that this was when the store cum offices were extended which parallels with the style of decoration applied.

The former offices, to the west of the complex, are of stuccoed brickwork and built on the street alignment. The building is parapetted, with gables to the sides and a cornice to the front and two stuccoed, corniced chimneys serve the internal rooms. The fenestration is asymmetrical which supports the evidence of the added bay of 1870-1.

The gabled store to the east is similarly built to the street frontage and has face brick walls in Colonial bond. An unusual (for the period) hit and miss brick vent occurs in the gable whilst the large opening at the north has been trimmed with what appears to be a former gate post; this may have been widened. The roof is of iron, and, by its pitch, appears always to have been.

These buildings are the material memorials to Robert Turnbull and his dominance of the port's development from its beginning. The complex is one of the earlier warehouse groups in rural Victoria.

37. *Former Post Office,  
Wharf Street,  
CA 2/2B  
National Trust Classified FN 2732.*

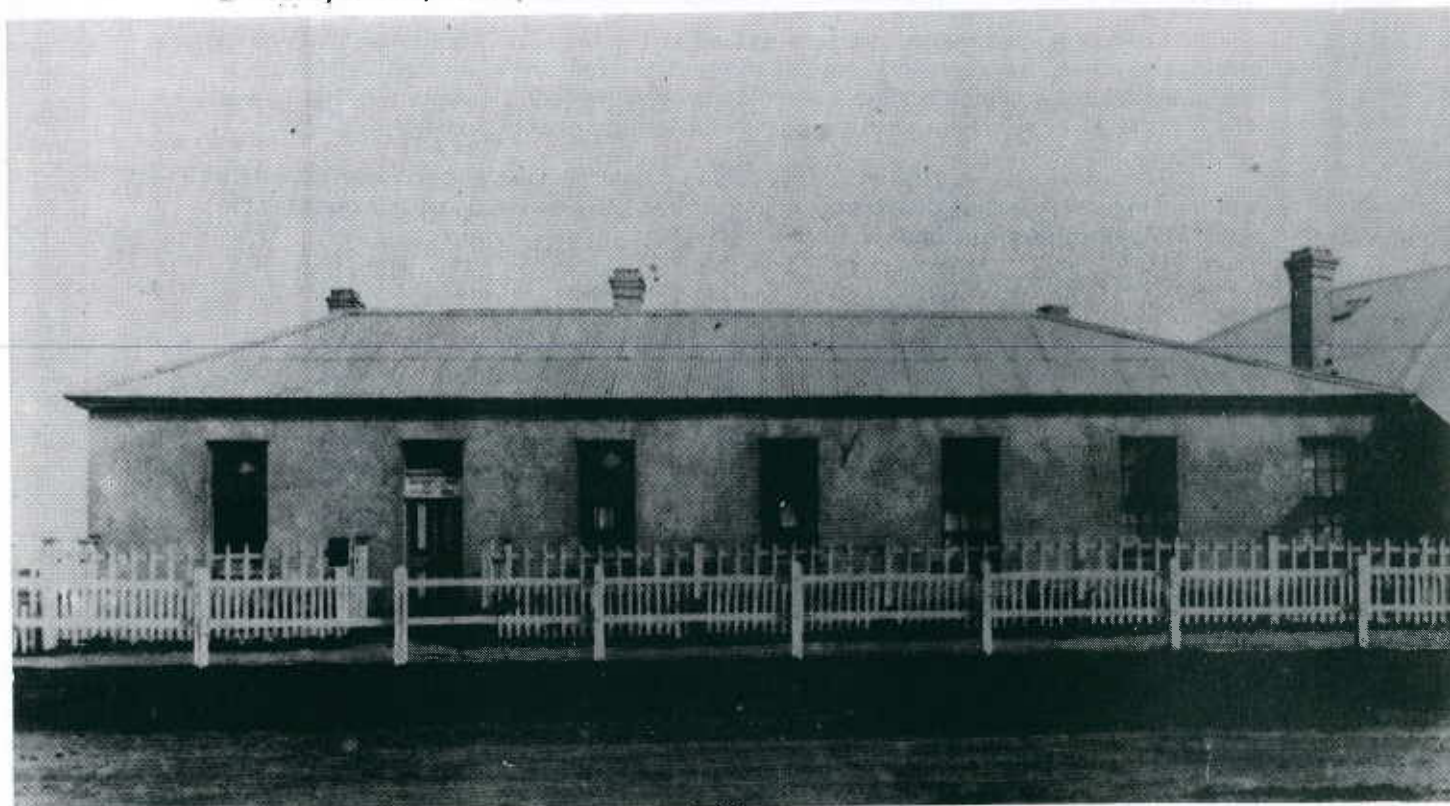
*Public Purposes Zone*





■ 38: House, former Derwent Hotel

■ Formerly as Receipt and Pay Office (Port Albert Maritime Museum)





Luttrell who was a boatman in the pilot service, had been listed as residing in Port Albert in 1856. 3 Five years after the purchase of the property, in 1857, he mortgaged the house and land to William Pearson of Kilmany Park. 4 By 1863, he had sold the house to William Brown who was a boatman in the pilot service. 5 Brown owned three such houses in that year, in a row along Wellington Street; all leased to the harbour department. 6 In 1866 he leased this house to Charles McIntyre of the department; whilst retaining ownership into the 1890's. 7 In later years, fishermen occupied these dwellings; continuing their maritime association.

Although in a state of serious disrepair, this double-fronted timber cottage is of great importance to Port Albert. Originally of two rooms and a verandah, other rooms were added to the rear progressively under the distinctive high hipped roofs synonymous with shingling. Rare half-round guttering, supported on unusual carved timber brackets survives in the roof space on the extension whilst the later, but still typical, ogee-profile gutter exists on the exterior, suggesting an early date for the extension, perhaps in 1862, when Luttrell mortgaged the house.

The verandah posts are of an unusually large section and support fretted timber friezes executed with a primitive boldness. Internally, the lining is hessian over split hardwood lathes whilst the front wall is of beaded softwood boarding. Slim architraves of 70 mm. exist in most rooms of the house.

It may date from as early as the extension of the pilot service in 1847, or the provision of a Collector of Customs in 1844 and remains as the sole survivor of a group of houses inhabited by the officers of the Colonial Government Customs and Harbour Departments during the 1840's.



41. General Store,  
Former Port Albert Bakery,  
Tarraville Road,  
CA 2/4,

Commercial A Zone

First allotment owner: John Sydserff, 18/9/1855, 150 pounds.



Captain John Sydserff purchased the allotment from Robert Turnbull in late 1855. 1 By mid 1861, he conveyed the property into a trust for his wife Mary, and their children. 2 A building existed on the lot at this date whilst Thomas' survey of 1854 does not show any improvements. 3 The first rate entry on the building of 1863 shows that James Flanner leased it as a "House and shop" from Sydserff. 4 Previous to this, James J. Hart, the former Tarraville schoolteacher, had owned this business; having sold it to Flanner in mid 1858. 5 Hart's business was that of baker and general storekeeper with additional services such as Liston's Tarraville Dispensary being available and, with it, the services of Dr. Hedley were also available. 6 Hart had called for tenders to supply the bricks to construct the store in January 1856 and had titled his business then as the Port Albert Bakery. 7

Captain Sydserff, the owner of the land, was also the Commander of the *Storm Bird*, a Port Albert Steam Navigation Company steamer and this probably was the reason for his presence in the port since the Company had commenced. 8 His command fully occupied him, particularly during the wrecking of the *Thistle* in 1859.

Sydserff sold the store to William B. Howden later in 1875 for 120 pounds: 9 Howden being the owner occupier of the store, with his wife Emma, until Robert Howden became the owner c1896 10 when he leased the store to William and Julia McKerrow. 11 The McKerrow's ran the store until well into this century. 12

The old store is a stuccoed brick building with a steep, gabled parapet to the north side, two show-windows, a central entrance and a verandah. Early photographs show that the shop once had a gabled parapet to the south, which has been broken away and a central parapet entablature which also has gone. The original verandah had four, stop-chamfered posts, with block capitals, and a gently concave roof spanned back to the parapet. Beneath the verandah, the openings and trims seem basically original although the insertion of postal private boxes has disrupted one show window. 13

The store was once part of a dense row of shops, some of which have been demolished, and provides an almost original example of what form these shops took. As well as having origins from the maritime dominance of the town, in the 1850-60 period, the store also had a mariner owner. Built in 1856, the building may be the oldest functioning store in Gippsland.



■ 42: Art Gallery

■ 43: Gowrie House



44. *Maritime Museum,  
Former Bank of Victoria,  
CA A of 1/5*

*Commercial A Zone*

*First allotment owner: Bank of Victoria, 8/8/1859, 350 pounds.*



The *Gippsland Guardian* of 28th March, 1856, announced the opening of a branch of the Bank of Victoria at Port Albert on 31st March 1856. This was in a two-storey prefabricated iron store building in Victoria Street, near the corner of Albert. Subsequently the bank procured a more central site and architects, Robertson and Hale, designed the present stuccoed brick building for an opening in 1861. <sup>1</sup> Finishing touches were added when tenders were called for the painting of the bank exterior in March 1862. <sup>2</sup>

As a result of the widespread depression of the early 1890's, rationalisation of bank premises took place such that the last Manager, Duncan Jamieson, closed the Port Albert branch in October 1895. <sup>3</sup> The Bank of Victoria which had been introduced to Victoria in 1853, was taken over by the Commercial Banking Company of Sydney in 1927.

In 1896, the building was sublet to Robert Hunter and a baker, John Lewis, as two residences. <sup>4</sup> A teacher, Richard Trembath was resident there in c1903-6 and Henry Avery, a fisherman, leased it in c1910. <sup>5</sup>

The bank is a complete example of early conservative Classical revival architecture. The splayed corner, quoining, corniced parapet and pedimented entrance provide a well composed and fittingly grand corner building for a township which was intended to grow well beyond its eventuality. Of the work of Robertson and Hale, only one other is known to have survived; being the Beechworth Bank of N.S.W. (1856-7) which is a two-storey version of the Port Albert bank; being similarly sited on a corner. <sup>6</sup> Generally, externally and internally, the bank is original with the exception of the external colour scheme; the adjacent building to the north is also disruptive.

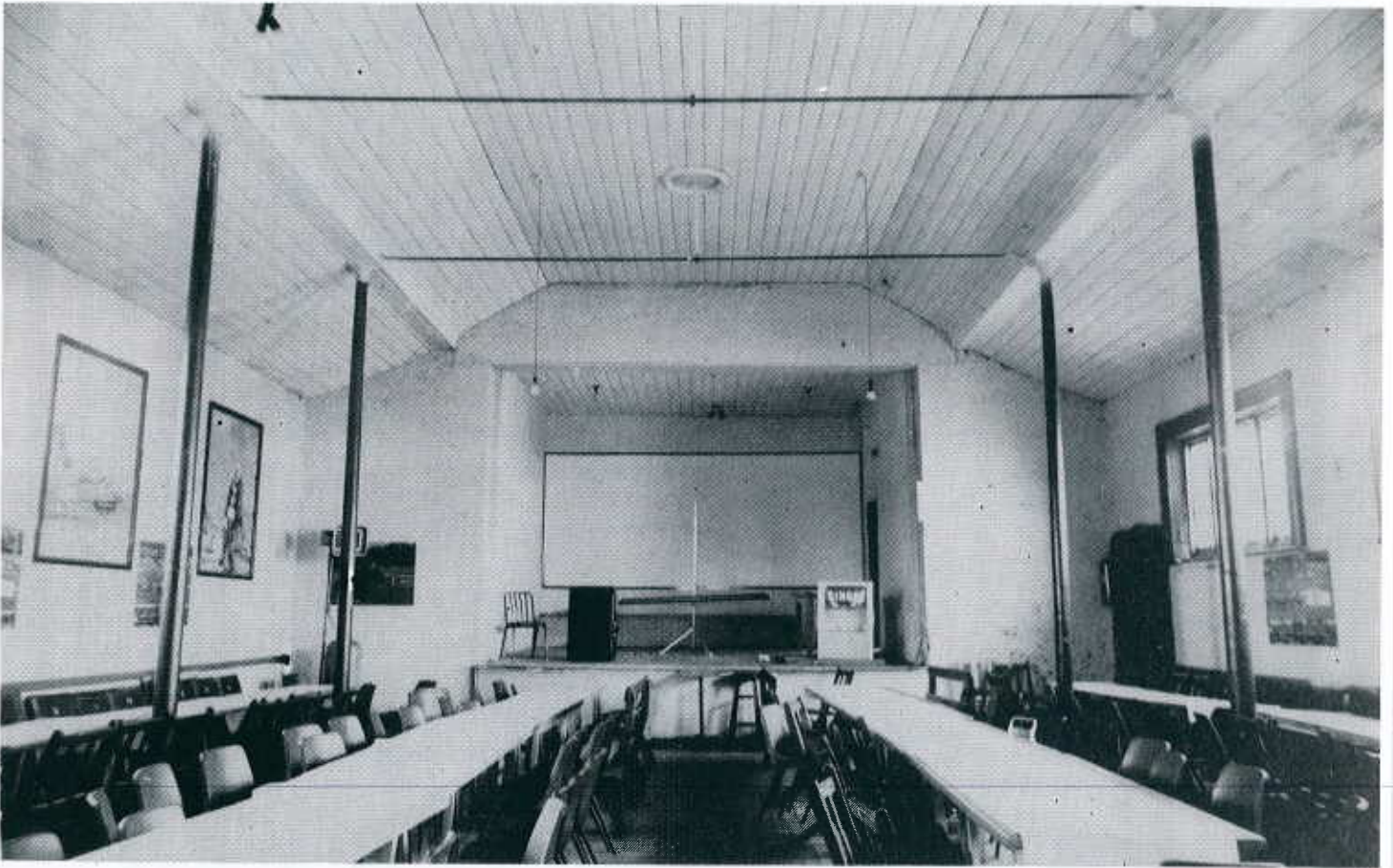
The northern house of the pair has been altered further; the windows have been enlarged, the shallow bays destroyed and the door replaced. A picket fence survives on both frontages and the face brick chimneys occupy identical positions on each roof.

The house is contributive to this commercial section of Tarrville Road being of a row-house form and sympathetic to the shop rows opposite despite its residential use.

Historically it was, however, part of a second residential precinct in Port Albert which followed that commenced in Wellington Street. The high pitched roof of Captian Thomas Spring's residence (c1860) once echoed that of the Flannery, to the north. The Flannery is all that survives of this residential segment.

46. *Hall,  
Former Mechanics Institute,  
Victoria Street,  
CA 35/4.*

*Public Purposes Zone*



Reputedly built in the 1880's, little remains of this Mechanics Institute Hall except what might be seen in the west elevation. A gabled slated roof, extends over a central doorway and an oculus is placed above it as a hint of the stylistic prepossession of the original brick building.

Internally the ceiling is of softwood boarding now painted, in a gambrel profile, which is supported on plain cast-iron columns: generally apart from the paintwork, the interior is near to original.

Subsequent additions to both sides, the stucco facing and the lower addition to the front have devastated this building.

47. House,  
Albert Street,  
CA 2/4B, formerly 24/4.

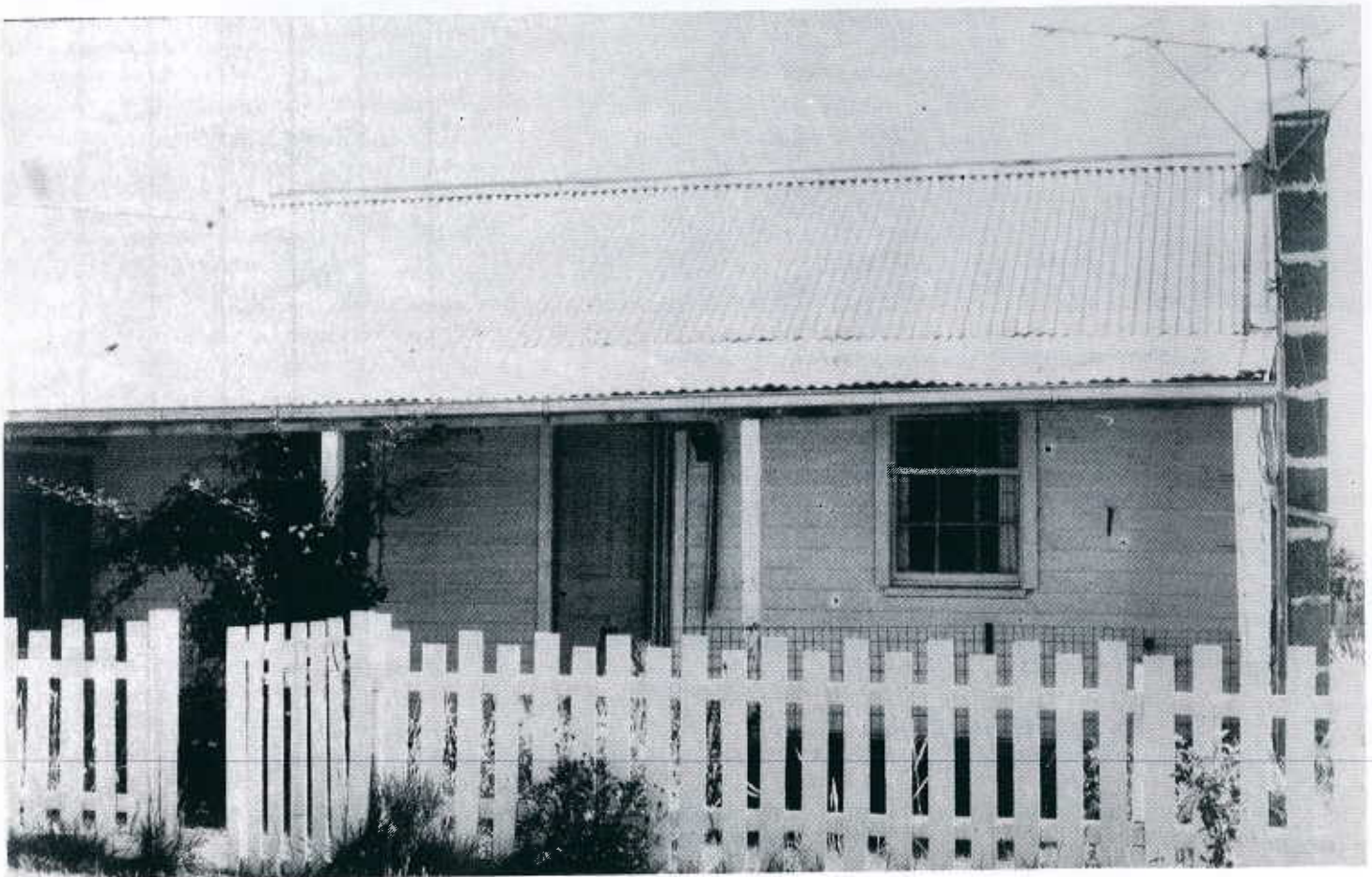
Residential Zone

A substantially altered timber cottage with the characteristic hipped roof and chimney shape (rendered) of early buildings in the town. It contributes marginally, with the sympathetic roof form, however, the alterations determine that it is of little architectural or historic importance.

48. Wee Waa,  
House,  
Queen Street,  
CA 15/5

Residential A Zone

A simple, double-fronted timber cottage with a verandah, six-pane window sashes and a high gabled roof which was reputedly owned by a Mr. Hay during the 1930's. The square-head picket fence completes the details of a house which, although probably built c1910, contributes with its simple form to the built character which was Port Albert.



- Raglan* 49. St. John's Anglican Church,  
Nelson Street,  
CA 16A/10

*Residential "A"*  
Public Purposes Zone

First allotment owners: William Belcher, Thomas Burrows, Richard Kelly and Jacob Ferris, December 1862, 10/-

The Rev. W. Bean commenced Anglican services in the Port Albert Customs House in June 1856. 1 Upon Bean's departure in 1859, the Rev. Stretch arrived in 1860 and urged for the construction of a church at the port, whilst having the use of the Presbyterian Church in the interim. 2 The first Port Albert Anglican Church, which was designed by architect, H. B. Thomas, 4 was opened in March 1863, to be burnt by bush fires in January of 1884 during the



to the Education Department for 200 pounds (1964) who had originally acquired land nearby in 1870 for the Port Albert school.

The church is the second oldest in the study area and was the first Wesleyan church in South Gippsland. Although simply executed it, with Christ Church Tarraville, are the two oldest known timber churches in Victoria. Typically, a mature *Cordyline australis* stands near the door; exemplifying a hardy survivor from planting of the nineteenth century.

### TARRAVILLE TOWNSHIP

52. *Christ Anglican Church,*  
*Tyers Street,*  
*CA-3-6/1, 1a, 2.15*  
*Reserve Gazetted 29/5/1855*

*Residential A Zone*  
*(presumed public purposes zone)*

The Rev. W. Bean obtained both donations from public subscription and a Government grant of 456 pounds (from the Melbourne Diocese) for the erection of this timber church which had been reputedly planned in brick many years before, but the labour and materials shortages during the gold rush period had forced a more modest structure. This was Gippsland's first church when it opened on June 8th 1856. <sup>1</sup> Architects, J. W. Pettit and George Hastings executed the copious drawings for the church in August 1855 and S. Taylor was its builder. <sup>2</sup>

The church was constructed from 228 × 50 mm. sawn timber members joined with mortise and tenon and waterproofed with an iron water bar at these joints. Horizontal slabs drop between the uprights formed with the cladding, whilst the roof was clad with split shingles which have since been covered with sheet iron. The church was painted externally and fenced in 1859. Softwood lining boards were fixed to the interior walls in 1868 at a cost of 17 pounds. <sup>3</sup> The whole church was designed with elements from the Northern European Gothic era, adapted to timber construction to provide a picturesque building which differs in detail only from the original drawings; a free standing belltower instead of the belfry drawn at the roof apex and minor changes in the porch. Two subsequent churches at Port Albert were to adopt a simplified version of this church, although they were not designed by the same architect(s), H. B. Thomas and George Hastings being the architects (49). <sup>4</sup>

Internally the church has the old pews, oil lamps and brackets oil chandeliers and the minute detail of finish not associated with remote rural churches. A miniature quior, the free-standing porch within the body of the church and the fretted trefoil tracery of the window



■ 52: *Christ Anglican Church*

■ 53: *House, Bridge Street*





the new act in 1873. The present brick building was erected in 1877: the design by the Education Department's architecture branch, under Chief Architect, Henry Bastow. 3

It is a simple, gabled building, built in English bond brickwork and appointed with a gabled porch. Like Muckleford South Common school of the same period this school was elevated on standard plan No. 8 and possessed little design content. 4. The building is generally original, except for the unfortunate painting of the brickwork and the probable deduction of a trussed gable and finial from the end elevations. 5. Presumably the school yard has been re-fenced and the roof re-clad. Mature pines and gums exist in the yard. This is the most substantial building in the government township and the third oldest.

## WORANGA PARISH (Reeve Special Survey)

55. *Ruinous House,* *Agricultural Zone*  
*Loughnan Street, (Allotment 203 x 107L, west of Reeve and south of*  
*Loughnan Streets, Section 23)*  
*First allotment owner: Thomas Smith, 30/8/1856.*



This property belonged to Thomas Hannibal Smith, the storekeeper, who was rated for it during the period 1863-74. 1 Smith was the proprietor of the Tarraville Emporium from c1851-2 and the Gippsland Store of the 1860's, 3 this was his residence whilst the store was further east. 4 His shop and residence were cited in the 1856 electoral roll: this, plus the listing in the 1851 Directory, indicated that the building was there prior to the sale, on lease. The house and the business were conveniently close to both the Commercial and Tarraville Hotels and was part of a secondary retail area to the main thoroughfare of Reeve Street.

as a guiding star in his advocacy of the wants of the district . . ." (after McMillan's election to the Legislative Assembly). 5 Some measure of Hedley's popularity may be gained from the Alberton Roads Board's first election in 1858, where Hedley was voted chairman with 109 votes; the nearest contender being John Gellion with 84. 6 He was on the committee of management to the Alberton Horticultural Society, convened a meeting to form the first Mechanics' Institute in South Gippsland (becoming its President) and was elected to the Assembly after McMillan's resignation in 1860. After a number of frustrations, Hedley also resigned in 1862. 7

The house is distinguished by the panelled supports to the verandah which, although not complete, are rare. From the verandah, double half-glazed doors open into the main rooms whilst the windows are similarly of a double casement form; all are typical of the fine proportions of early joinery. Arched ventilators, set in the gable, have been closed in, but the roof shingles remain under the sheet iron.

Additional wings lie to the east and south and parts of the verandah have been enclosed, however, the house itself is remarkably untouched. It is one of five structures surviving from the period of freehold sales on Reeve's survey at Tarraville and one of three on the survey itself. It was also the home of perhaps South Gippsland's most distinguished resident of this period.



■ 57: Farmhouse

survey and, unlike (57) where the house design probably represents an architect's influence or hand, the buildings here are of simple concept with distinctive aspects (gutter brackets) not found elsewhere in the State. The complex is of great historical and architectural importance, particularly to Gippsland as representative of its vernacular and to the State as representative of a well preserved rural complex, outside of the larger pastoral lease type of homestead.



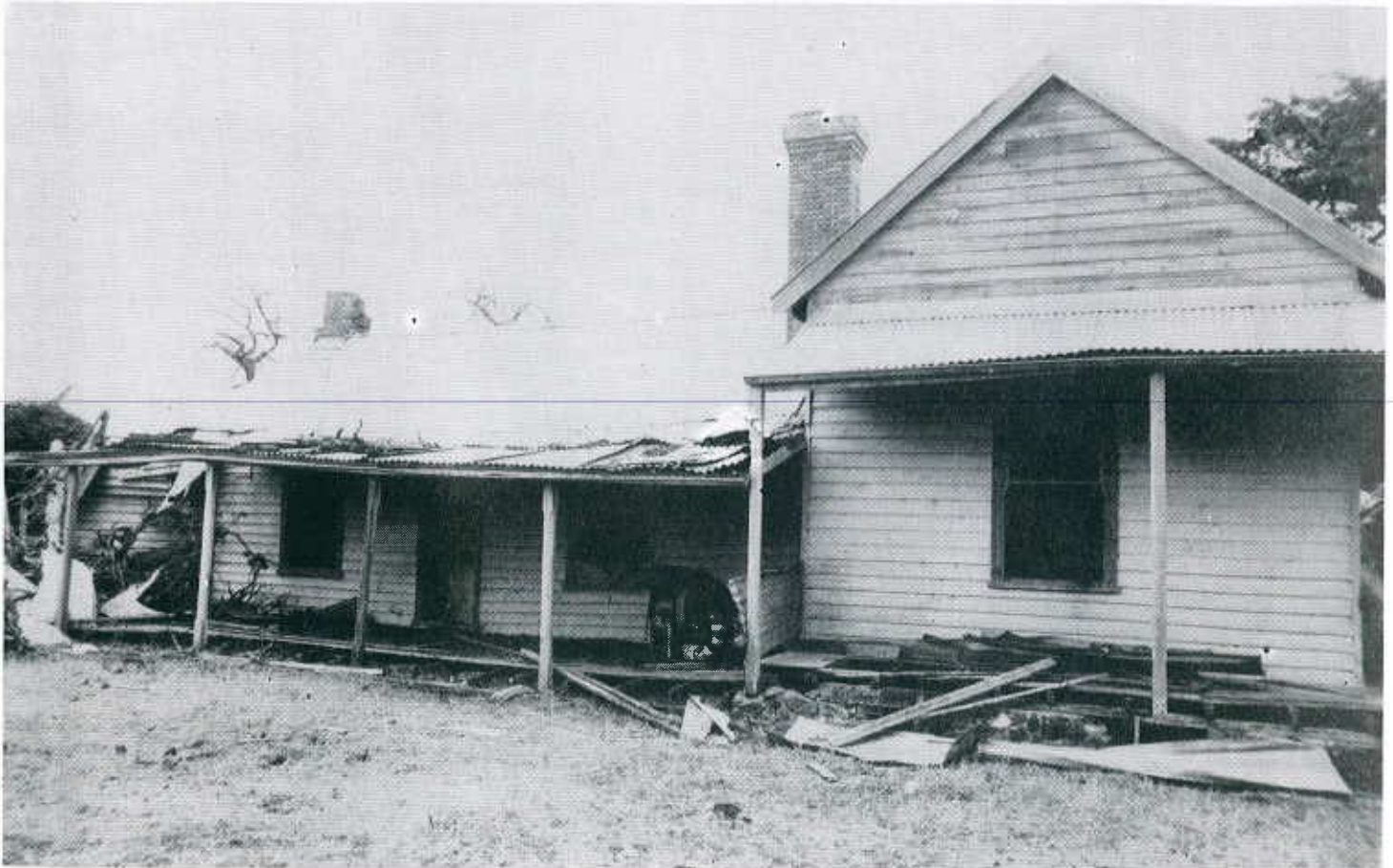
■ 58: Green Hills. West Wing: east elevation, verandah added c1920

■ Eastern Wing: earliest on the site





■ 59: Hawthorn Bank. Interior Kitchen



provide the grounds for hair-reinforced mud with presumably a manure sealing mixture applied over all. Most of this has been clad with hardwood boards externally with the exception of under the north verandah. The roof has the steep double-gabbling but the expected shingling as exists on the first homestead section has been removed, and the roofing timbers appear to have been replaced. The floors are of consolidated earth with a sealing layer and the walls have been partially lined with softwood internally. The recent fall of a large pine has damaged the south-east corner of the building as it has almost demolished the south wall of the homestead. The old homestead is of a similar round, sappling wall framing, but the roof frame is sawn. Shingles clad the roof whilst lathes provide grounds for internal plastering to the walls and ceiling.

Throughout all the buildings, gas brackets have been distributed in the days when a gas manufacturing plant existed on the property; the roses have survived. The later section of the homestead has been decorated in the period c1910 with Arts and Crafts pattern wall papers, whilst plaster roses survive from an earlier time. Around the complex are several mature exotic trees which include Moreton Bay Fig, Monterey Pines, a Norfolk Island Pine, a Blue Atlas Cedar and a Hoop Pine: most of these are in good condition and all are of significant age. They are an important if thinning, setting for the house.

Little definite may be said of the age of the two early structures, or whether one predates the other, (i.e. primitive construction prior to the arrival of imported timbers or a pit saw in the district), or whether one was a detached kitchen and servants' quarters, built of cheaper materials. It appears that perhaps the latter is the case and that the old section is from the 1850's whilst the new wing was added c1887 to be renovated early this century. The rarity of wattle and daub construction and the evident building sequence of the complex make it of historic value to the area, as an example of homestead development over a period, of perhaps, sixty years.

60. *Farm Complex,  
Shaws Road,*

*Agricultural Zone*

*Allotment: CA 7 (Subdivision of a former Government Reserve, west of  
Reeve's Survey)*

*First allotment owner: F. G. Dalgetty.*



# SIGNIFICANT AREAS

1. *Old Port, Foreshore reserve (Public Open Space Zone) south of CA's 38-43 (Agricultural Zone).*

## *History:*

Between them (Tarra and Albert River channels), is the government township reserve of Alberton . . . *the old settlement being at the beach (situate in relation to the township of Alberton much as Liadet's (sic) to Melbourne) and containing some dozen huts, a good inn and store, and more to the eastward, a point of land used as the shipping place of the port – denominated the "Stock Yard" (Port Phillip Herald, 14/11/1843).* 1

With the formation of the Gippsland Company in 1841 came the application for a Special Survey, in the name of John Orr, (later gazetted in June 1841). 2 Then came the news that the company was about to establish a town and Melbourne merchants were invited to journey on the *Corsair* and *Sea Horse* to view the site, along with prospective settlers and supplies. Reeve and Wentworth took the journey only to be informed on reaching the Old Port of Governor Gipps five mile radius reservation, gazetted in March, 1841. Despite this, about one hundred persons had begun to erect tents and small cottages along the beach: awaiting the government survey of the Special Survey boundaries which would allow settlement to proceed. 3

By August 1841, the company's stores and goods were offered for sale and by December, the settlement was "verging on starvation". 4 Relief came when the Van Dieman's Land cattle trade commenced early in 1842. 5 Meanwhile, surveyor Townsend resumed his measurement of the Special Surveys and reported that the land around the settlement was "useless, barren ... covered with scrub and swamps". 6

Once the survey design for Alberton was completed in mid 1842, the government allowed the sale of 180 acres at Shipping Point to Major Alex Davidson in September 1843, leaving the Old Port settlement as illegal occupation until land sales alienated the area in 1858. 7 Thus when Tyers (Land Commissioner) arrived in January, 1844, he sought licenses from these "squatters" which they did not care to pay. Meanwhile, Turnbull, Orr and Co. had commenced to erect a store at Shipping Point by March 1844, 8 and apparently shifted the "frame-house" from the Old Port to act as the Port Albert Hotel, thus taking the economic base from the beach settlement whilst sales of lots at Victoria had taken place in July 1843. 9

H. B. Morris journeyed with surveyor Robert Russell, described it in 1843 as 8-10 slab huts roofed with bark, two stores and a shop, 10 but G. H. Haydon noted in December 1844, that a few deserted and delapidated huts existed at the Old Port and that a heap of bottles showed the site of a public house. 11

Robert Russell's water colours of May 1843 show a flagstaff at least two boarded houses and a number of huts set in a clearing on the water's edge. 12

M. J. Campbell's Government Survey of 1856 was the long awaited alienation of the reserve providing a line of small allotments at Old Port Beach which faced a proposed three chain road reserve. 13 They were distributed around the starting point of the dray track to MacAlister's station which was perpetuated in the survey as a straight road to Myrtle Point: this was the track blazed by Angus McMillan after his arrival there in February 1841. 14

Campbell noted that the vegetation was *Tea Tree scrub on sandy ground, with honeysuckle gum, and "thick Gum scrub"*. 15 Today, Coastal Banksia (honeysuckle), *Eucalyptus viminalis* (manna gum) and *E. Ovata* (swamp gum) are evident as is Coast Beard Heath (*Leucopogon parviflorus*), *Acacia oxycedris*, Coast Salt Bush (*Atriplex cinerea*) the *Bossiaea cinerea* and the *Acacia oxycedrus*.

The vegetation of 1856 exists today as does remnants of the old settlement of 1841-4: broken bottles and English size bricks are evident, scattered on the beach and inland. Excavations around the site presumably mark the activity of bottle searchers.

## *Significance:*

This settlement, as apart from pastoral occupation, was the *first in Gippsland and the next after Melbourne, Williamstown, Geelong and Portland* in Victoria. The site is of historic importance to the State.

## *Preservation and Enhancement:*

Preservation of the site would involve retention of the following elements: the vegetation stated and the remnants of the occupation. Enhancement of the site would require development control of allotments 38-43 to ensure retention of the above elements.

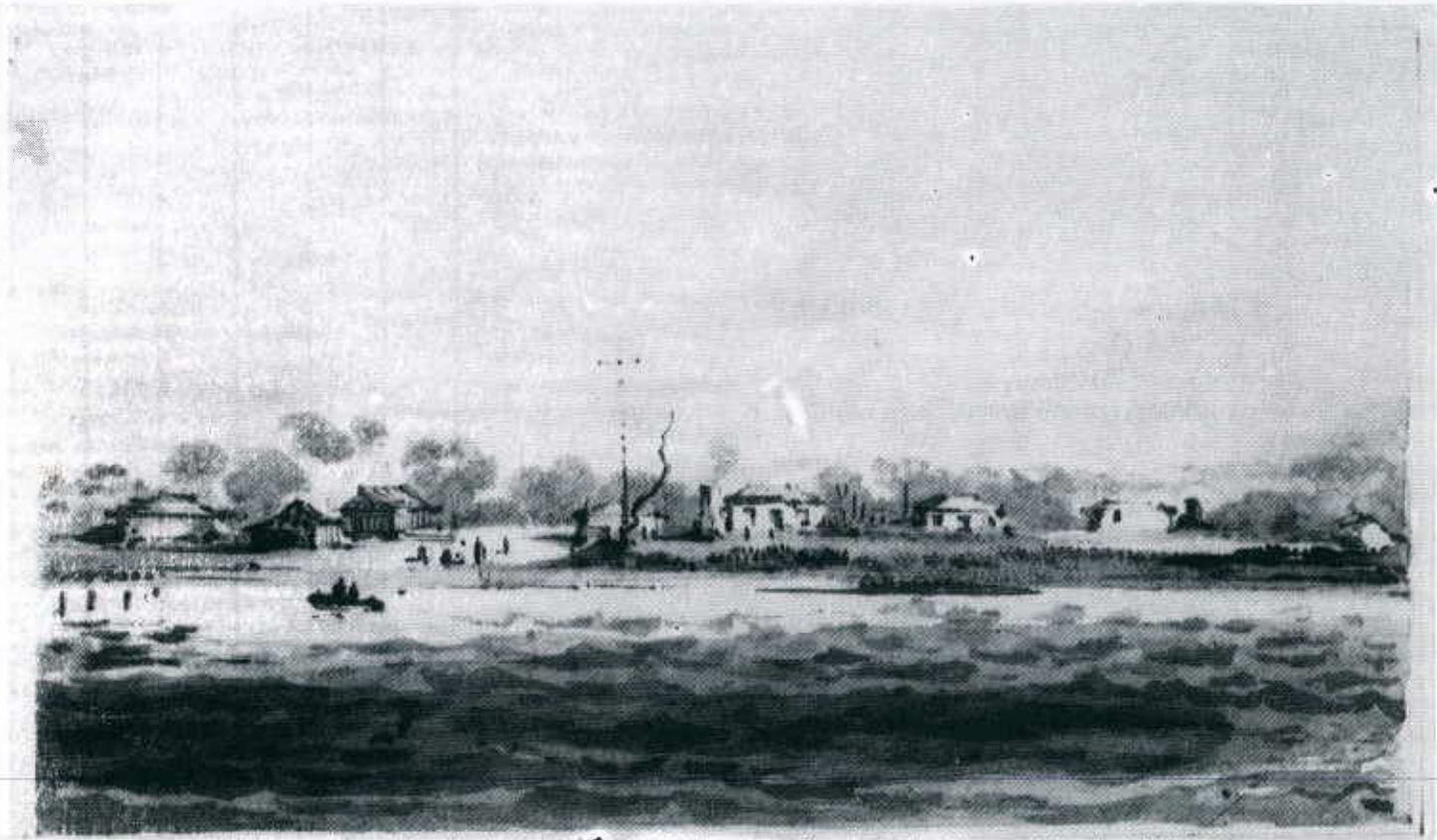
*Existing Controls:*

Zoning under the *Shire of Alberton (Coastal) Planning Scheme 1962* in respect to Reserved Land allows consent to construction of "shelters, kiosks and the establishment of camping areas and other structures..": requires consent for damage, removal or destruction of trees; allows building required "for or incidental to the purpose for which the land is reserved". This relates to the foreshore reserve. In respect to the Agricultural Zone, adjoining, consent may be given to construction of a house on existing allotments over 0.5 ha. and consent is required for constructing any works 100 m. from the Albert River (southern part of allotments). No provision is made for control or removal of vegetation.

*Recommendations:*

A defined area encompassing the remnants of the settlement should be declared an *area of special significance* which would require a permit for any alteration to the existing physical state of the site.

Council should oppose adverse possession claims over CAs 38, 39 & 41-43; investigate Council or State acquisition of CAs 38-43; encourage State compulsory acquisition of CAs 40A, B; and ensure inclusion in proposed Nooramunga Fisheries and Wildlife foreshore reserve. Council should locate present brick, glass remnants on detailed re-establishment survey of CAs 38-43.



■ Water colour sketch by Robert Russell, 1843 (Latrobe Collection, State Library of Victoria, MFN H6286, 27751, 1608)

*Note.*

Since the study's completion, the Alberton Shire approached a maritime surveyor, Harry Reed, of the Queenscliff Ports and Harbours Hydrographic Station, to execute a re-establishment survey of the Old Port. Basing the settlement's position at the cross roads shown on Townsend's survey plan, Reed used traditional instruments and techniques to verify its site. This point coincided with the old chimney remnants and bottles and traces of a jetty were sighted. These findings locate the 1841 settlement ruins on CAs. 38, 39 south of McMillan's Monaro track and extended along the shoreline track to the new port. Perusal of Townsend and Campbell's field notes, from their two early surveys of the area, would further establish the original extent and authenticity of the ruins.

Revised recommendations concerning this site would include public acquisition of CAs 38, 39 and further investigation (archaeological and historical) to determine the extent of the original village and additional land acquisition.

1851) Atkinson and Reeve preferring to lease allotments. 5 Hence the allotments sold in Victoria were the first registered freeholds to be given, outside of the existing towns of Melbourne, Williamstown, Portland and Geelong, in the Port Phillip district and may have arisen from the sixth town survey in the state.

Similarly, after Portland (Tyers and Townsend survey town lots January to February, 1840, land sales October), 6 Melbourne (land sales June 1837), Geelong (land sales February 1838), and Williamstown (land sales June 1837-), came to Alberton's town design. Surveyor, A. S. Townsend, designed Alberton and submitted the plan to the Executive Council in July 1842. 7 The New South Wales Government Gazette of September announced the existence of the town, predating Victoria. It was chosen as near a good berth, near fresh water and near the non-swampy arable land to the north. However, land sales did not occur until 1846, thus allowing the minor boom on 1842 – to establish Victoria as the major township of the two. Thus Alberton was the fifth township designed and surveyed in the State.

Of Tarra Vale Village and Port Albert, both of the associated government towns (Tarraville and Palmerston) followed much later and were thus never competition despite government investment in Palmerston. Tarra Vale appears to have been laid out at a similar time to Victoria, in 1843, and substantial buildings were constructed (two large stores by November, several large buildings under way in June 1844 to provide a total of 50 buildings and one "good inn" of brick). 8

Tarra Vale was on the overland track which reached both the pastoral leases and later, the gold at Omeo. It was considered that Tarra Vale was Gippsland's foremost town of the 1840's: 9 having more substantial buildings, both private and business premises, than Alberton, Port Albert or Victoria in the pre-gold period (by 1848-9, 100 lived in the town alone) 10 Tarra Vale was perhaps the seventh town laid out in Victoria.

Port Albert commenced with Davidson's purchase in 1843 11 and plans of 1848 and 1845 show streets and allotments existing at the corner of Wellington and Victoria Streets. 12 Tyers reported in July 1844, that there was 10 acres of useful land and the rest was swamp: 36 people lived there, carting water 13 from the wells at the Old Port or the Tarra River, near Tarra Vale. By December, he noted that, the scrub had disappeared and "neat cottages" had emerged from the Tea Tree (see Building 39, Wellington Street). 14 H. B. Thomas, surveyor and architect, prepared a further subdivision with that shown in 1848 incorporated, ready for the first land sales in 1854. 15 Some 14 private houses were shown outside of Turnbull and Co.'s stores, Gellion's Port Albert Hotel (1844-) and the boiling down works along Bay Street. The government town of Palmerston followed cries of monopoly and land sales ensued in 1858. 16

With all of the above towns, it is evident that, at no time, did the surveyed town ever fully materialise. Main access roads such as Wharf Street, Reeve Street and Turnbull Street became strip development centres and residential use radiated from these centres; the unit land area quickly increasing, particularly in Victoria and Alberton.

From an early date, allotments were acquired and amalgamated unofficially such that when E. T. Newton purchased the site of Eabon Eabon in the 1870's, he purchased several suburban allotments, *en bloc*, and the result was the loss of the road pattern of Russell's 1843 survey. Similarly, many sites in Port Albert, Victoria and particularly in Tarra Vale are without known owners. Single owners in Port Albert have acquired whole sections and could devise amalgamation of the allotments within.

#### *Existing Buildings:*

Of the remnant buildings, preservation has been recommended in the light of the above historically important factors of each town (particularly Tarra Vale). Where these towns materialised in strip development, as in Reeve Street, and subsequently the materialisation has decayed, it is recommended that buildings which might appear derelict or of little architectural importance be considered as possessing the essence of the historic importance of the early township concerned, given that the street pattern may not be retrievable.

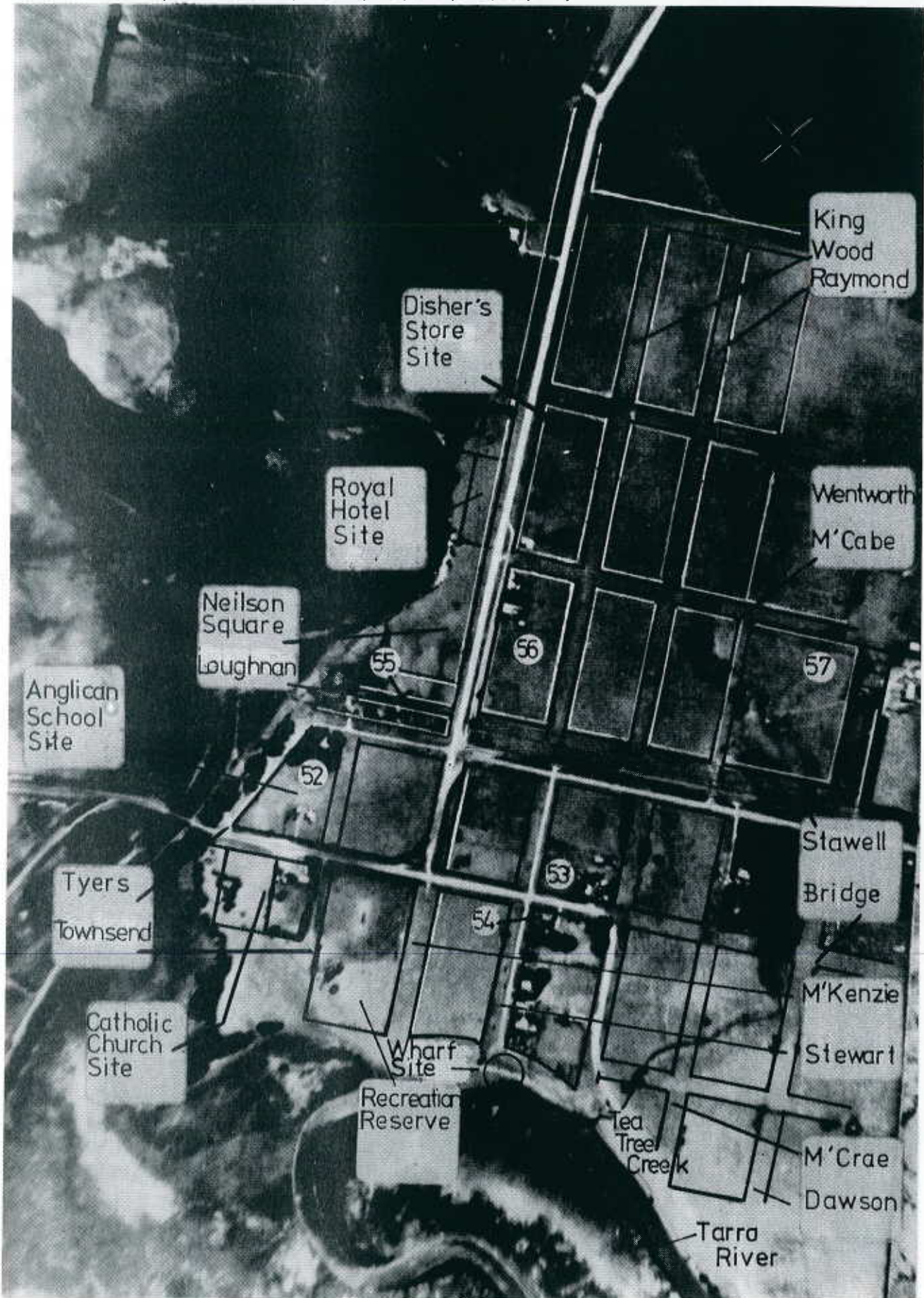
Historically, "key" buildings, such as hotels, churches, shops and the homes of persons widely known in the district (Dr. Hedley) are considered to be of relatively greater importance in epitomising the town's form or identity than other building types; given a broader scope for identifying with the larger number of occupants or users of the building and the larger number who knew of its existence. Other supportive buildings which do not date from the early periods, but resemble those that do, are recommended for preservation also.

#### *Existing Street Patterns:*

Of the allotments and the streets themselves, many are no longer identifiable. In Tarra Vale, only Reeve Street exists where subtle topographical indications only show



2. Tarra Vale Village, Tarraville Township  
(Source: RGO Plan; CPO T 70, 1855; W 219, 1855; T 72, 1854)



bagged brickwork, painted within a specified colour range, for walls and painted corrugated iron or slate for roofs; the use of simple hipped or gabled roof forms; the adoption of symmetrical front elevation fenestration; and the option to add a post verandah. These would apply to new buildings, alterations to designated buildings, additions and advertising signs. Controls could also provide for flexible useage for the existing buildings in the town, where their future viability may depend on a non-conforming use.

However, given the low availability of sewerage disposal and the cost of extending this, together with a likely oversupply of town lots from the old surveys, it is more practical to encourage adoption of building form guidelines and the relevant National Trust of Australia (Vic.) Technical Bulletins (signs, paint colours, historic gardens, infill development).

This option implies protection, by permit, of only the designated buildings. Given that they comprise a minority of the potential housing lots, it might be argued that to seek concurrence with the original development plan is unrealistic and unachievable, given the low demand for house lots there: thus putting unnecessary restrictions on those who do seek to reside in Alberton or Victoria. The intervening space between designated buildings may always remain rural and they may not be "swallowed" by intrusive development.

*Port Albert - Palmerston:*

Existing controls are predominantly in the form of Residential A Zones. Hence no permit is required to build a house on an existing town lot. The Commercial A Zone, which follows existing useage along Tarraville Road and Wharf Street, requires a permit for all new private building works. This also applies for the Industrial A Zone which extends east of Duke Street.

*Recommended Controls:*

As with Victoria-Alberton, an area of special significance would overlay the zoning pattern with similar requirements, relating to new and existing buildings. Here the impact of controls would be felt more, given the large residential zone; however these controls would serve the dual purpose of amenity control and historical conservation of a potentially desirable area for retirement housing. Because of the relative containment visually, of the two towns by the Tarra and Albert Rivers, uncontrolled development would have a more apparent effect on the town's historic environment and its attraction for prospective buyers. So too is there a greater density of designated buildings than Alberton-Victoria.

*Tarra Vale - Tarraville:*

The more historic Tarra Vale is almost entirely Agricultural Zone and hence only new houses on lot sizes over 50 ha. do not require a permit. The exception is the site of the now demolished Mechanics' Institute which is zoned Special Use: again a permit must be obtained for all buildings.

Tarraville is zoned mainly Residential A west of McCrae Street with an Industrial A Zone on section 7; the latter zone requiring a permit whilst the former does not, for house construction.

The historic importance of Tarraville is not considered to be high enough for blanket development control outside of the designated buildings. However, Tarra Vale possesses great importance, both as an early town design and the possessor of two buildings of equally high importance, with the nearby Christ Church reinforcing this. The town could become an *area of special significance* in the Planning Scheme, requiring permits for all construction, as is the case now.

In all cases, amalgamation of allotments in these historic towns, should be resisted to prevent absolute destruction of their street pattern and density.

If an *area of special significance* is not sought: all permit applications in the zones should be considered by the Responsible Authority, accounting for direct or indirect impact on the historic environment.

3. *Turnbull's Bridge/Brewery Road*  
*Part C.S. 15, 18, 19 Alberton East*  
*Part allotment 35, Orr Special Survey*  
*Part Reeve's Special Survey*  
*Part C.S.8, Woronga*

*Agricultural Zone*

*History:*

This area contains both man-made elements, historic sites and extensive areas of natural vegetation. The Alberton watering place was located south of the bridge in 1851 whilst nearby, on the Victoria to Tarraville track, was the Rev. Willoughby Bean's "parsonage" occupied by him from c1848 until his departure from the area in 1858. It was



reported as destroyed by fire in May 1861. <sup>2</sup> Turnbull's Bridge is shown on plans as early as 1851. <sup>3</sup> The original track to McAllisters station from the Old Port crossed south of this spot from when McMillan blazed it in 1841. <sup>4</sup> Later township communication extended and reinforced this track until the survey of Alberton East parish "straightened" part of it, for a road to Myrtle Point, in the 1850's.

This bridge appears to have been renamed Carpenter's Bridge (Carpenter built it), when it was rebuilt along with the road to Victoria in 1857. <sup>5</sup> On the other side of the Tarra River, north of the track and next to the river, was Gilbert Brown's Tarra Creek brewery which was built on a site which only became freehold in 1857 for a price of 20 pounds. <sup>6</sup> Yet in August, 1847, the brewery had opened its extensive premises including a "brew house, loft and two apartments occupied as dwellings plus a detached Malt Kiln." <sup>7</sup>

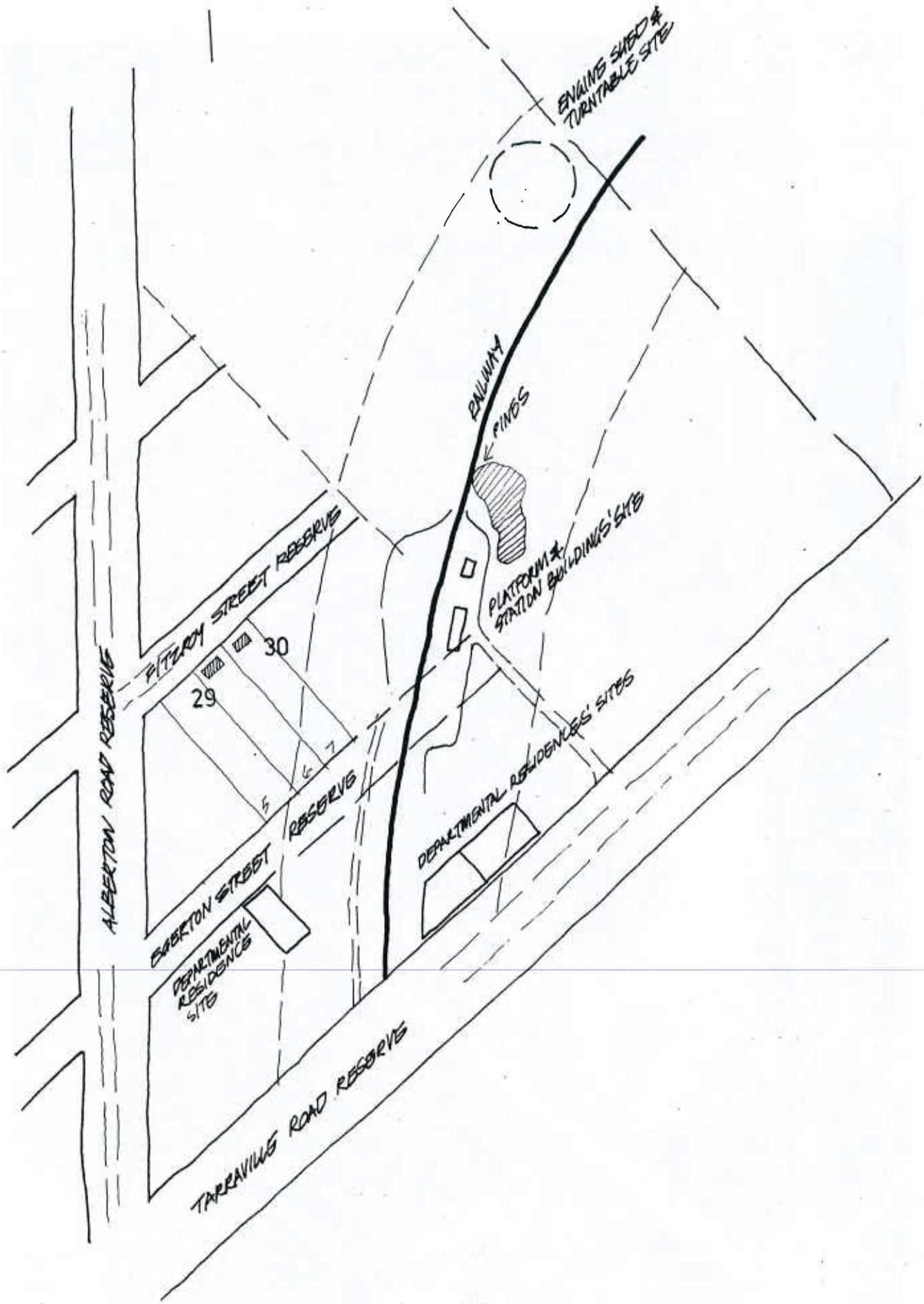
The vegetation was described as "dense scrub with the upper areas timbered with 'Gum, Cherry tree, Lightwood and Honeysuckle and Wattle' ". <sup>8</sup>

Today Bean's parsonage has left no trace, but a clearing exists in the thick scrub where the Victoria-Alberton watering place once existed. The narrow bridge decking and balustrading is typical of this century, whilst the rough log structure under could date from any time, c1850-1930 and thus perpetuates a timeless approach to bridge construction, using unsawn timbers connected roughly into a trestle frame. Thick strands of willows occupy a small part of the western section of the brewery site, whilst *Melaleuca ericifolia* (Swamp Paperbark), species of *Lemptospermum* (Tea Tree) and some *Goodia lotifolia* are evident: the first being represented in a profuse number of mature specimens. Some planting still gives indications of the brewery site boundaries.

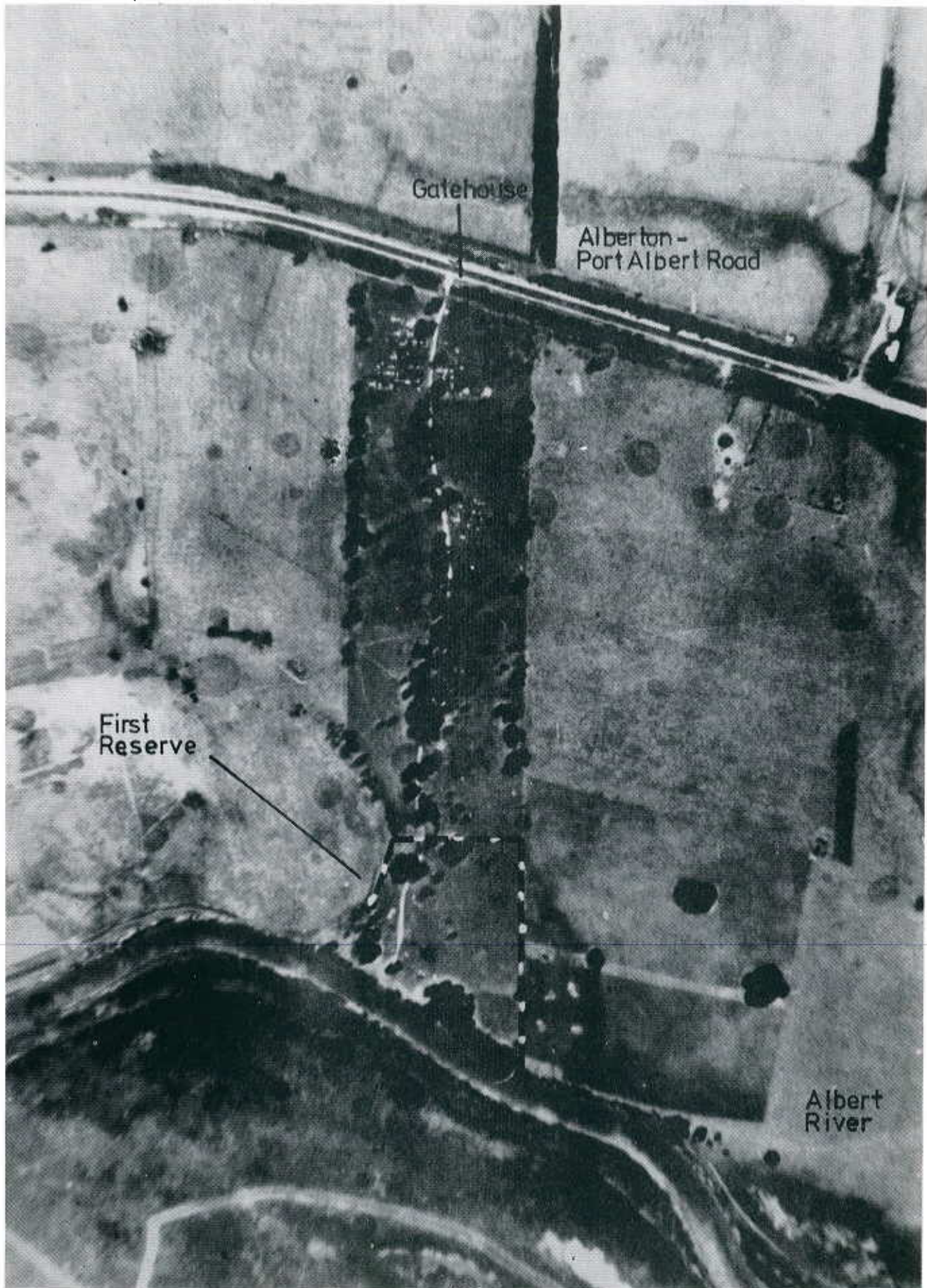
The area is evocative of vegetation prior to pastoral occupation and is the site of McMillan's 1841 crossing of the Tarra River, Brown's brewery of 1847 (as in Brewery Road) and the first Anglican clergyman resident in Gippsland, the Rev. Bean's "parsonage" of c1848. <sup>8</sup>

#### *Existing Controls:*

Agricultural Zoning applies in the area, presumably to the water's edge. All building work and subdivision over 50 ha. (minimum) are subject to a permit but consent may be given, under certain conditions, to allotments as small as 0.5 ha. and a house built thereon (with cognisance of "natural features"). Only fences may be built within 100m. of a stream



5. *Alberton Cemetery*  
(Source: CPO, A9, 1856; A 14B 1847)

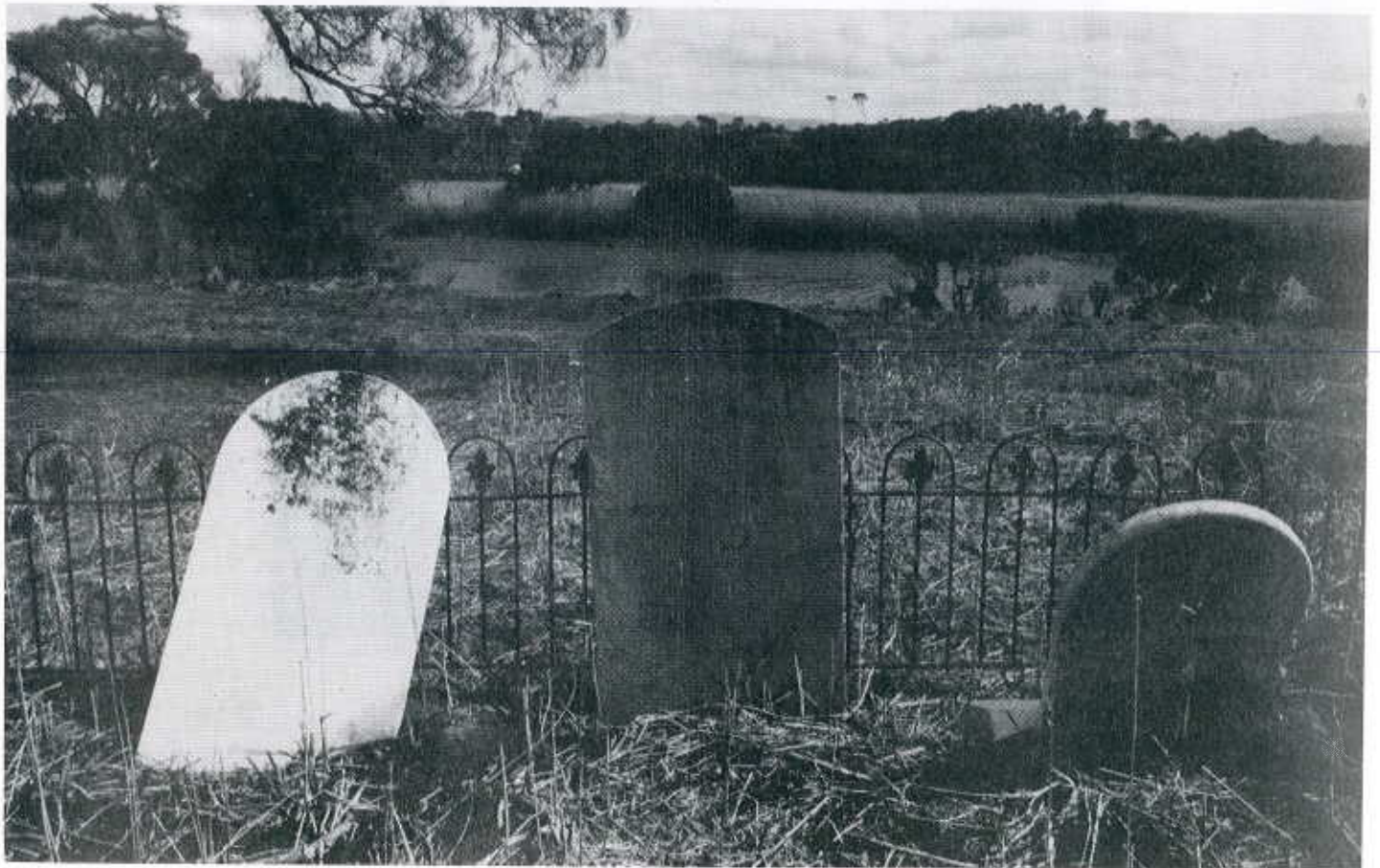


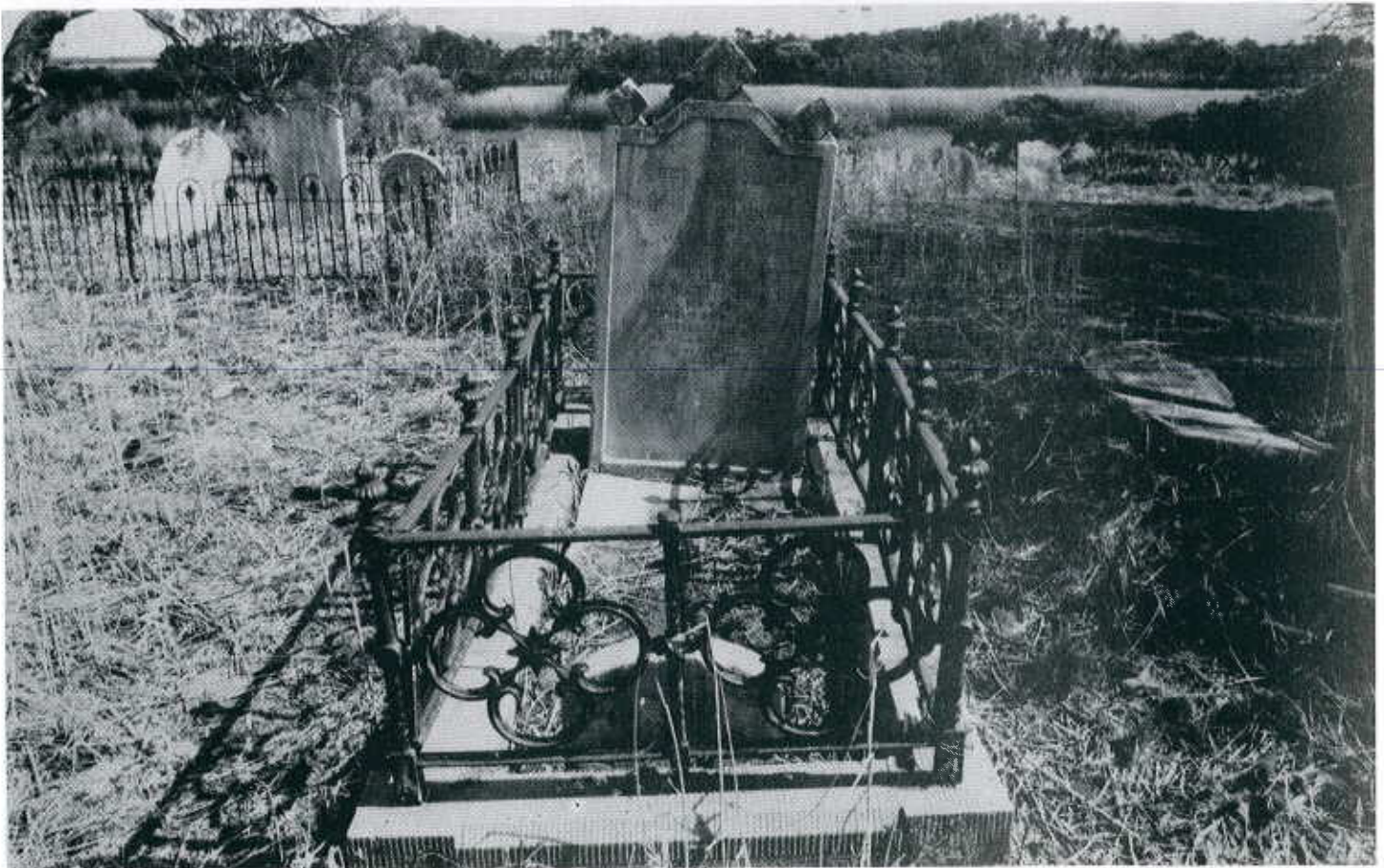


■ Gatehouse, probably designed by George Hastings

■ Margaret Anderson (d. 21.1.1877), wife light keeper; mason, Marsh Grout & Co., Molb.









6. *Myrtle Point/Tannery Road*  
*CA 28, 105, 30 Alberton East*

*Agricultural Zone*

The area contains three building complexes of great historic value to the area (Refer Buildings 18, 19 and 21) which date from the alienation of the Crown Lands; Wood Cot Park being the most important and the most strategically situated next to the Tarra River. In 1856, the area was described as ... "very good land, cherry tree, wattle gum and sheoak", whilst at the north-west corner of the adjoining CA27 was the watering place for the Port, in 1851. 1 Hence the appellation given the road, constructed from Port Albert's centre line, as a "water road". As well as Buildings 18, 19 and 21, Neilson had also constructed a house (now demolished) on the fifth corner of this intersection (CA106A), to create a relatively built-up area, taken together with the tanneries which developed further west along Tannery Road (Eisert and Schoenke). 2 The indigenous vegetation which remains is similar to Area 3 (Swamp Paper Bark); being confined to the river bank. The balance is exotic grasses.

Perhaps, because of Neilson's residence in the area, it obtained a desirable residential quality suitable for the establishment of the Anglican parsonage there. When Wood Cot Park was offered for sale in 1861, the position it held was cited as "not equalled in the district". 3 The character of the area is made more evident by its confinement by the bend in the Tarra River, ... Myrtle Point.

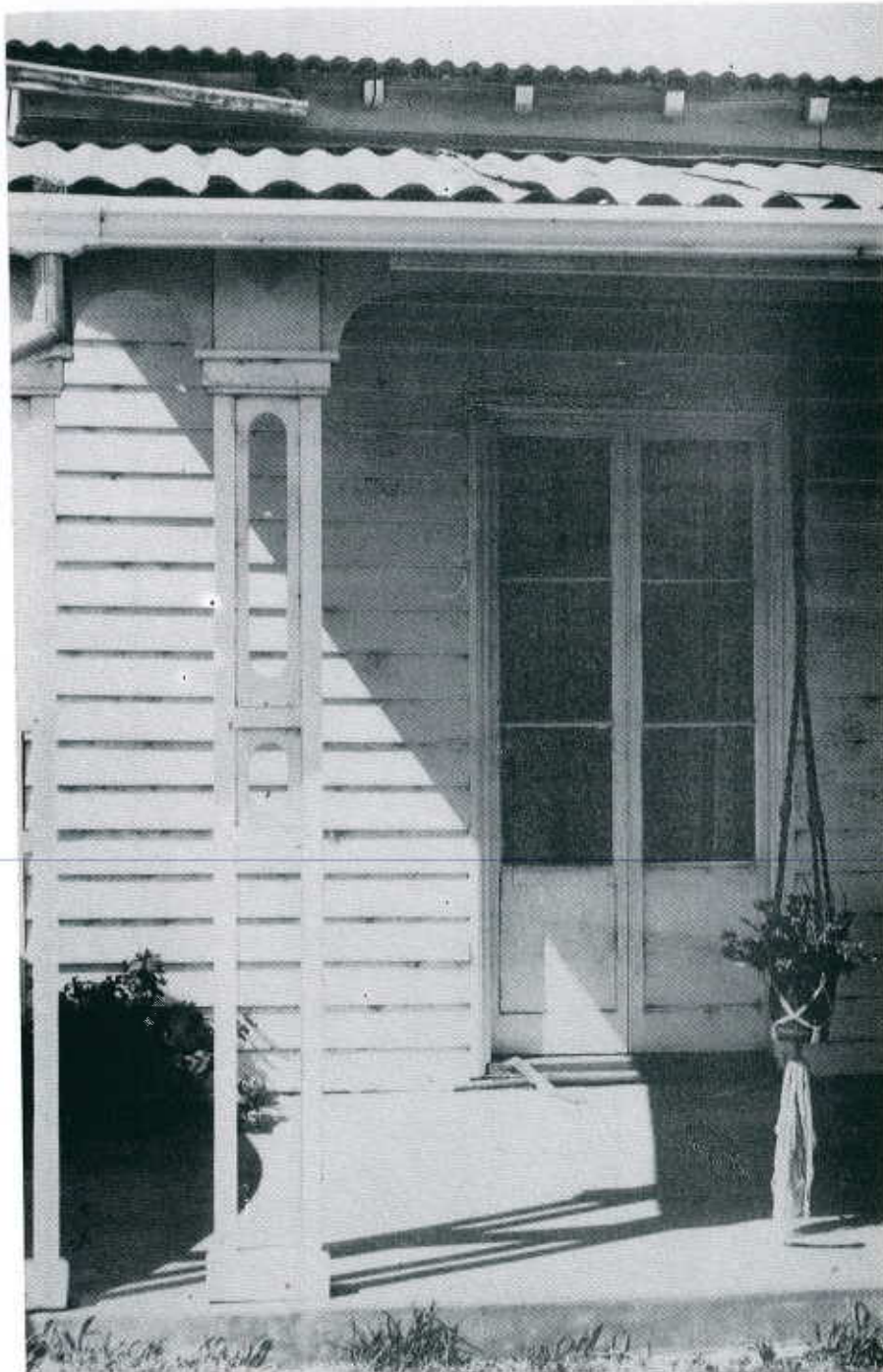
The Agricultural Zone provides that, permits are required for houses on areas less than 50 ha. (c124a): as none of the allotments specified is greater than this, amalgamation would be required. It is considered that if the Responsible Authority accounts for the historic characteristics of the area in considering any greater density of house lots, and the designated buildings require planning permits for alteration, additions or demolition that the area is sufficiently protected.

- (a) Weatherboards (front wall) 150 x 20 mm. softwood with 130 revealed; the edge is beaded 15 from lower; (side walls) 120 x 25 sawn hardwood.
- (b) Architraves (interior, exterior) 45 x 25 softwood.
- (c) Bricks chimney, handmade 220 x 60 x 105 i.e. American "common" size or South British "1936 standard", set in eroded lime mortar (shells).
- (d) Verandah posts 120 x 200 @ 2360 ccs, 25 stop chamfer 300 from top; frieze 42 x 25.
- (e) Roofing split shingles (hardwood) under corrugated iron; 60 x 100 ogee profile spout with spike fixing to verandah; half-round, heavy gauge spouting on timber brackets concealed in roof, rear of first two rooms.
- (f) Internal-framing 70 x 45 studs @ 445 ccs.
- (g) Internal lining (front section) hessian and paper (repapered 1938) over split c150 x 20 hardwood grounds with c50 spacing between; 230 high skirtings; softwood flooring T. & C. 150 wide; softwood beaded board ceilings.
- (h) Fence (remnant) sawn arrow-head (100 high), hardwood 70 x 18. Note: broad pickets of hardwood (split) may also be seen at (28).



(57) *Hedley's House*  
*Tarra Vale (c1854-9)*

- (a) beaded edge doors
- (b) narrow architrave against exposed jambs
- (c) beaded edge softwood boards
- (d) nails hand forged, exposed heads
- (e) timber frieze and panelled support
- (f) broad gauge iron verandah roof
- (g) beaded edge fascia board
- (h) original timber brackets at roof edge for missing half-round gutter (see 39)
- (i) new concrete floor and plastic verandah gutter



- (j) fine toplight with transfer to glass
- (k) bifocated ½ glazed door, new hardware
- (l) panelled and glazed side lights

### Shop Front Details

- (42) *Art Gallery*  
*Tarraville Road, Port Albert (1859)*  
Typical pilaster frame to windows and door in timber.



# DEVELOPMENT GUIDELINES

## **Buildings:**

The intent is to encourage the form and finish to new buildings and additions to existing buildings to be in harmony with the identified buildings listed. This applies particularly to Port Albert-Palmerston.

**Front Setbacks:** should be greater than adjoining identified buildings (if detached housing); should align with existing buildings (if attached or semi attached commercial buildings, specifically Tarraville Road, Port Albert, west side between Raglan and Victoria Streets).

**Building Form:** building form and bulk should be near to that of adjacent identified buildings; 2 storey only in Tarraville Road as above. Facade or frontage width. This width may be applied to succeeding bays of the new building if larger than the prototype adjacent identified building(s); flat facades are preferred.

**Roof form** should be from illustrated types found in study area; if adjacent to parapetted roof line (Tarraville Road), a parapet or simple hip roof type (D) should be adopted: eaves should be not greater than 300.

**Pillar street verandahs,** skillion or concave roof profile should be adopted (as U.B.R. 3601, 3601 (b) ); Council should assist building owners in application for amendments, as required, to the Building Regulations Committee to achieve historically sympathetic pillar verandahs.

**Building Materials:** roofs should be corrugated sheet metal, slates, asbestos or timber shingles; Walls should be stuccoed or bagged masonry, horizontal timber boarding (square or beaded edge preferred), chimneys may be bagged or stuccoed masonry or approved face brick.

Openings should be framed in timber.

**Fenestration:** Window placement should be symmetrical about an entrance on the front elevations (French doors included).

Window width to height ratio should be no greater than 1:2 except in shop windows.

Shop Windows should be multi-paned in character of (42) and (5).

**Colours and Signs:** May follow National Trust of Australia (Vic.) Technical Bulletins 2.1 and 1.0; all timber should be painted, not stained or treated.

**Decoration:** No period decoration (cast-iron etc.) should be attempted unless used in restoration where evidence supports its existence.

All proposals should be allowed the usual provision for appeal against refusal of a permit which may also be submitted to a selected advisory body.

*Refer following Diagrams:*

- 1 *Prevalent Form Types in Study Area*
- 2 *Illustration of Building and Setback Controls*



# BUILDINGS

1. House,  
Johnson Street,
2. House,  
Johnson Street,  
1. (a) RGO  
(b) RB 1889, 2178  
(c) 1886, 1275
3. Mareen,  
House,  
Bank Street,  
1. (a) G.G. 6/5/1856,  
L.A. Roll  
(b) RGO Unregistered  
Sale Note 20/12/1855  
2. (a) RGO Unregistered Sale  
Note 7/12/1866  
(b) P.O.D. 1868,  
3. (a) C. of V., V3, p.624  
(b) RB 1879, 236  
(c) RB 1885, 540
4. House,  
Bank Street,  
1. RB 1863, 160,  
NAV 10 pounds.  
2. (a) RB 1885, 551  
NAV 5 pounds  
(b) RGO  
3. RB 1885, 551
5. Store & Residence,  
1. RGO  
2. Ibid  
3. C. of V., V3, p.624  
4. RB 1870, 161:  
RB 1876, 218:  
RB 1882, 104:  
RB 1882, 109  
5. (a) RB 1910, 164  
(b) RGO  
6. Lennon p. 153f.
6. House,  
Streleski Street,  
1. RGO  
2. Ibid  
3. RB 1891, 2001  
4. Alberton Cemetery  
5. Verbal from owner
7. House,  
Kirksopp Street,  
1. RGO: 10.5.1861  
2. RB 1863, 436
8. House,  
Danger Street,  
1. RB 1903, 396
9. House,  
Turnbull Street,  
  
1. Verbal by present owner.
10. Victoria Hotel,  
Turnbull Street,  
1. (a) Irving, abstract of  
Licensing Records,  
Alberton District  
(b) G.G. 5/10/1855  
(c) RB 1863, 176  
(d) G.G. 28/9/1860  
(e) G.T. 16/3/1870  
(f) RGO: 11, 12/1,  
Victoria Town.  
2. RGO 222.394  
3. (a) RB 1889, 2194:  
NAV 120 pounds  
(b) G.S., 2/4/1890  
(c) RB 1890, 1906,  
NAV 180 pounds  
4. Photo, PAMM
11. Eabon Eabon,  
Hawdon Street,  
1. RGO: 222.394: no mention  
of buildings; acquires  
40 acres.  
2. RGO: 277.571: 'Together  
with all houses, fences,  
etc.'  
3. RB 1881, 213  
4. (a) POD 1880-1  
(b) Burgess Roll 1883  
5. Victorian Directory  
1851  
6. LA Roll 1856  
7. RB 1863, 303f  
8. RGO 134.988  
9. (a) G.G. 21/12/1855  
(b) G.G. 3/7/1857  
(c) President Roll
12. Former Alberton Butter  
Factory, Russell Street,  
1. (a) RB 1897, 66  
(b) RB 1899, 13  
2. Defunct Trading Co.  
File, PRO  
3. (a) RB 1905, 307  
(b) RB 1906, 245  
(c) RB 1906, 82  
(d) Wise, P.O.D. 1903  
(e) op.cit., 1905  
4. RB 1910, 239  
5. Refer Butler, *Buln Buln*  
(Drouin, 1979) p. 427f.
13. Farm House,  
Yarram-Alberton Road,  
1. (a) RGO D.37, 25/2/1846  
(b) RGO Subdivision Plan  
No. 2, Alberton Gippsland  
R. Bagot, 1861: 'sold to  
J. & W. Martin' (SLV)  
2. (a) RB 1902, 434  
(b) RB 1903, 245

23. House,  
Old Port Road,  
1. CPO, A/9  
2. RB 1863, 160  
3. RB 1874, 71  
4. RB 1880, 37  
5. RB 1874, 71  
6. (a) RB 1888, 1922  
(b) RB 1891, 1734  
(c) RB 1892, 1651  
(last entry for Dight)
24. Tarrawonga,  
Tarraville,  
Port Albert Road,  
1. RGO  
2. RB 1863, 418  
3. POD 1868, 1871-2  
4. (a) RB 1918, 246  
(b) RB 1910, 231  
(c) RB 1905, 233  
(d) RB 1890, 1811
25. House,  
Albany Street,  
1. Township plan  
2. RB 1863, 150  
3. RB 1885, 533  
4. (a) RB 1903, 146  
(b) RB 1909, 141  
(c) RB 1918, 138
26. House,  
Brisbane Street,  
1. RB 1863, 156  
2. Butler, 1866 Directory  
3. POD 1863  
4. RB 1906, 222  
5. Langhorne Collection (AS)
27. House,  
Denison Street,  
1. RGO  
2. Ibid  
3. (a) RB 1889, 2132  
(b) RB 1890, 1870  
4. Township plan  
5. RB 1918, 336f
28. House,  
Denison Street,  
1. LA Electoral Roll 1856  
2. Butler, 1866 Directory  
3. RB 1863, 7
29. House,  
Fitzroy Street,  
1. RGO, 19/8/1860:  
'Together with all houses':  
21/2/1862: 'all houses  
outhouses and buildings  
thereon standing and being'  
2. RB 1863, 75  
3. (a) RGO  
(b) RB 1901, 12  
(c) RB 1918, 11
30. House,  
Fitzroy Street,  
1. POD 1868,  
2. (a) RB 1871, 66  
(b) RB 1896, 227  
3. RB 1884, 158
31. Former Police Station,  
South Street,  
1. CPO Township Plan 1857  
Survey  
2. CPO Township Plan 1889f  
3. (a) NTA FN 3246: copies  
drawings 19/10/1886,  
signed by Bastow and  
McKerrow, with initials  
'E.W.'  
(b) Gov. Gaz. 1886, p3281
32. Former Immigration Barracks,  
South Street,  
1. GG. 9/1/1857  
2. G.G. 28/3/1856  
3. G.G. 6/2/1857  
4. G.G. 28/8/1857 p 1608  
Contract 481 'Day and Law'  
see G.G. 23/10/1857, 'Daye  
and Lowe'.  
5. G.G. 5/6/1857  
6. G.G. 15/10/1857,  
23/10/1857  
7. G.G. 21/5/1858  
8. G.G. 8/10/1858, p.1961
33. Former Powder Magazine,  
Bay Street,  
1. Saunders *Historic Buildings  
of Victoria*, p.253  
2. *The Heritage of Australia*,  
3/112, 162, 165, 173  
3. G.G. 1866, p357  
4. Op. cit. 1866, p.2332  
5. P. & H., HWJ 3266
34. Government Wharf,  
Wharf Street,  
1. Lennon p30f  
2. *Turnbull Letter Books*  
Vol 1, 28/9/1846, letter  
to Buchanan and Orr from  
R. Turnbull (SLV)  
3. Lennon, p.88f  
4. RGO  
5. Lennon p209  
6. PWD Summary of Contracts:  
1859, 170; 1861, 224;  
1863, 196; 1864, 12; 1864,  
349; 1875-6, 159; and  
1877-8, 74 (PRO)  
7. P. & H.: Contract Drawings  
1859-  
8. (a) RGO  
(b) *Plan of Subdivision of  
the Turnbull Estate*



42. Art Gallery,  
Former shop and residence,  
Tarraville Road,  
1. LA Roll 1856  
2. RGO  
3. G.G. 5/8/1859  
4. (a) RGO  
(b) G.G. 14/6/1861  
5. RGO  
6. G.G. 16/8/1861  
7. RB 1881, 268-9  
8. RB 1880, 75; RB 1879, 83-4  
9. Photo SLV H31681
43. Gowrie House,  
Former Shop and Detached  
Residence,  
1. (a) RGO  
(b) LA Roll 1856  
2. RGO  
3. RB 1871, 47  
4. RB 1874, 51  
5. RB 1880, 62-3  
6. 1891, 1863  
7. (a) Rev. Cox  
(b) RB 1880, 62-3  
(c) C. of V. V3, p633  
8. Photos  
(a) SLV H31680  
(b) PAMM  
(c) Alberton Shire
44. Maritime Museum,  
Former Bank of Victoria,  
1. (a) G.G. 2/3/1860  
(b) G.G. 7/6/61  
2. G.G. 7/3/1862  
3. (a) RB 1894, 1042  
(b) Notice of closure  
4. RB 1896, 410D-E  
5. (a) RB 1906, 427  
(b) RB 1910, 18  
6. The Heritage of Australia,  
on 16/11/1895, 3/175
45. Rodondo and The Smiths,  
Formerly Flannery, Duplex  
Tarraville Road,  
1. RB 1863, 140f  
2. RB 1869, 378  
3. RB 1871, 109, 42  
4. RB 1874, 39f  
5. (a) RB 1876, 4  
(b) RB 1879, 69  
(c) RB 1885, 675  
(d) RB 1886, 1275  
(e) RB 1880, 109  
6. (a) RB 1887, 1695, 1664  
(b) RB 1896, 388  
7. RB 1906, 107  
8. (a) RB 1918, 378  
(b) RB 1914, 386  
(c) RB 1912, 309
46. Hall,  
Former Mechanics' Institute,  
Victoria Street,  
1. LA Roll 1856  
2. RB 1863, 434  
3. (a) RGO  
(b) POD 1875  
(c) Butler, 1866 Directory
49. St. John's Anglican Church,  
Nelson Street,  
1. G.G. 27/6/56  
2. G.G. 10/8/60, 1/8/62  
3. *Jubilee Souvenir* 1913  
(PAMM)  
4. G.G. 22/8/62  
5. Rev. Cox  
6. Photographs, PAMM, A.S.,  
SLV H31679
50. Former Methodist Church,  
Tarraville Road,  
1. G.G. 23/5/1856  
2. G.G. 22/12/65  
3. *Ibid*  
4. (a) RGO  
5. G.G. 22/12/65  
6. *Ibid*  
7. E.J. Wood, loc. cit.
52. Christ Anglican Church  
Tyers Street,  
1. Clark, *The Church of Our  
Fathers* (Melbourne 1934)  
p.65f, drawings illustrated  
2. (a) *Ibid*  
(b) G.G. 3/6/56  
(c) G.G. 14/12/55  
3. (a) G.G. 11/2/59, 25/3/59  
(b) Clark, loc. cit.
53. House,  
Bridge Street,  
(Robertson Beach Road)  
1. (a) RB 1894, 719  
(b) RB 1898, 345  
(c) RB 1905, 372  
(d) RB 1918, 405
54. State School (SS 615),  
Stewart Street,  
1. *Vision and Realisation*, V3  
p 1181  
2. Burchell, *Victorian Schools*  
(Melb. 1980) p57f  
3. (a) Op. cit. p92  
(b) *Vision and Realisation*,  
loc. cit.  
4. Burchell, pps. 71, 74  
5. *Ibid*
55. Ruinous House,  
Loughnan Street,  
1. RGO Two lots: the house  
203 x 107L; the store  
160 x 107L  
2. G.G. 16/1/57  
3. *Butlers, 1866 Directory*:  
under 'Leiton Street',  
equivalent phonetically to  
Loughnan Street  
4. *Ibid*  
5. (a) Butler, 1866 Directory  
(b) Lennox, pps. 181f, 166f
56. House,  
Former Commercial Hotel,  
Reeve Street,  
1. (a) RGO  
(b) Conveyance and  
Mortgage Memorials (AS)  
2. (a) 1851 Directory  
(b) G.G. 18/7/56  
3. (a) Conveyance and  
Mortgage Memorials (AS)  
(b) G.G. 22/4/59  
4. G.G. 18/4/62  
5. G.G. 13/6/65  
6. Lennox, pps. 154f. 166f.

# SIGNIFICANT AREAS

1. Old Port,
  1. Quoted by Rev. Cox
  2. Rev. Cox cites gazettal
  3. (a) N.S.W. G.G. 4/3/1841 p321
  - (b) Rev. Cox cites: *Port Phillip Patriot* 9/8/1841 and Brodribb
  4. *Port Phillip Patriot* 13/12/1841
  5. *Port Phillip Patriot* 21/2/1842
  6. Rev. Cox
  7. Rev. Cox cites: correspondence to Survey General; report in Port Phillip papers of sale 26/7/1843
  8. Lennon, p. 159
  9. Rev. Cox
  10. Lennon p147
  11. Op. cit. p149
  12. Picture Collection (SLV) MFN542: H6286, 27751, 1608
  13. PRO; A-8-A-6
  14. Ibid
  
2. Township Surveys
  1. Rev. Cox cites: N.S.W. GG 21/1/1841, 28/8/1841
  2. Lennon, p37
  3. Rev. Cox
  4. RGO
  5. Ibid
  6. W.S. Logan, *Victoria's South Western Ports 1834-1900* (Thesis, Melb. University 1967), p84f
  7. Rev. Cox cites: N.S.W. GG 27/9/42, p.1433
  8. Lennon pps 58f, 154f
  9. Op. cit. p.166
  10. Ibid
  11. Lennon p.158f
  12. CPO: A-11-A, A-13
  13. Lennon p160
  14. Ibid
  15. (a) Copy plan (AS)
  - (b) *Argus* 30/10/1854
  16. Lennon p161
  17. Township Plan, 1888 (SLV)
  18. Vale, V2 p.150 (SLV): CPO, T-70,
  19. CPO: A-9, A-8
  
3. Turnbull's Bridge,
  1. CPO: A-2
  2. GG 17/5/1861
  3. CPO loc. cit.
  4. CPO: Special Survey Plan 1
  5. GG 3/7/1857
  6. (a) RGO
  - (b) GG 30/8/1861
  7. Lennon, p173f
  8. Vale V2, p150 (SLV)
  9. Rev. Cox
  
4. Railway Reserve, Port Albert,
  1. L. J. Harrigan, *Victorian Railway to 1962* (Melbourne, 1962) p.287
  2. (a) Vic. Rail Contact Books
  - (b) Vic. Rail Station Yard Plans
  
5. Alberton Cemetery,
  1. CPO A14B, 6/1/1847
  2. Rev. Cox: photo of headstone
  3. G.G. 1860, p958
  4. G.G. 13/2/1857
  5. G.G. 3/7/1857
  6. Alberton East, CA92
  7. G.G. 3/12/1858
  8. G.G. 17/2/1860
  9. G.G. 8/6/1860
  10. G.G. 13/2/1857
  11. G.G. 13/12/1861
  12. G.G. 18/7/1862
  13. CPO, A-8
  14. Layout plans held by Alberton Cemetery Trustees, by George Hastings 1895; gatehouse not shown.
  15. C. of V. V3, p623
  
6. Myrtle Point:
  1. CPO A-2, A-9
  2. GG 25/6/1858, GG 30/4/58
  3. GG 16/6/61





